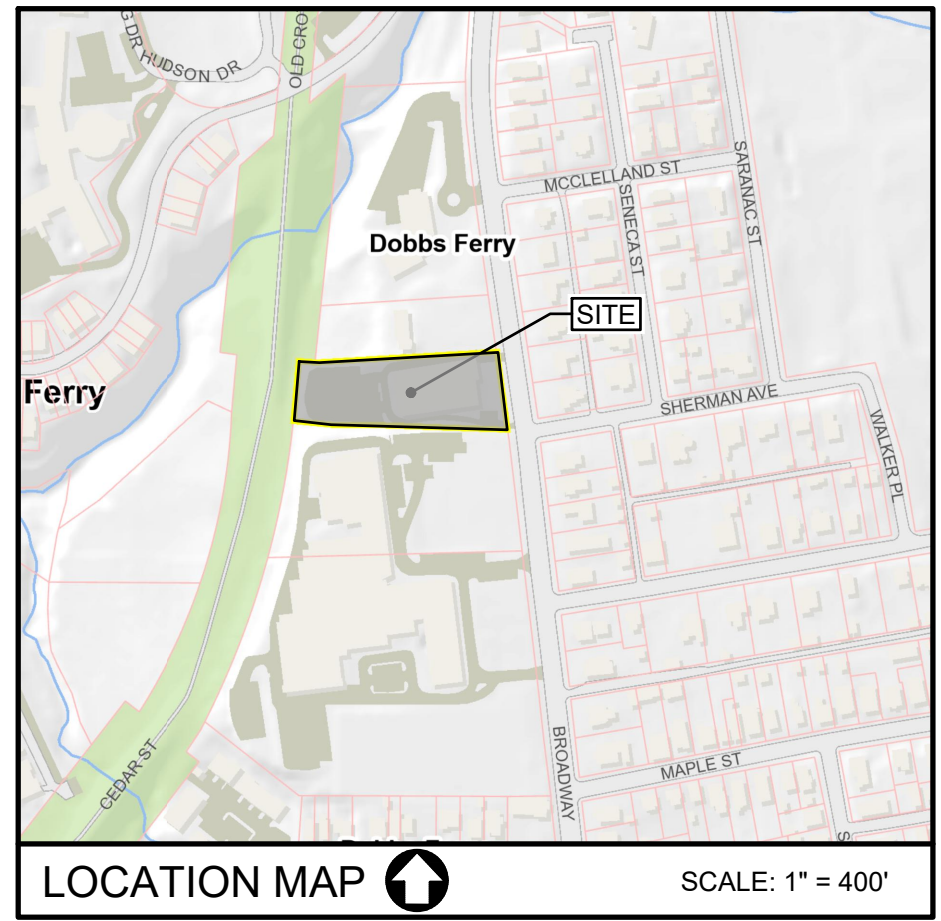


GENERAL NOTES

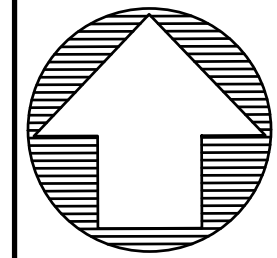
1. PARCEL TAX MAP DESIGNATION: SECTION: 3.40, BLOCK: 4, LOT(S): 11
2. TOTAL AREA OF EXISTING LOT: 61,747 SQ. FT. (1.42 ACRES).
3. SURVEY & TOPOGRAPHIC INFORMATION SHOW HEREON IS BASED ON A SURVEY PREPARED BY GABRIEL E. SENOR, P.C. ENTITLED TOPOGRAPHIC MAP, TAX LOT 11, BLOCK 4, NO. 515 BROADWAY (GHC), LOCATED IN THE VILLAGE OF DOBBS FERRY, TOWN OF GREENBURGH* DATED NOVEMBER 15, 2022.
4. PARCEL IS LOCATED IN THE VILLAGE OF DOBBS FERRY OF 1 (ONE-FAMILY RESIDENCE) ZONING DISTRICT.
5. PARCEL IS LOCATED IN THE POCANTICO & SAW MILL RIVER BASIN.

SITE PLAN
FOR
GREENBURGH HEBREW CENTER
515 BROADWAY, DOBBS FERRY, NEW YORK

LIST OF DRAWINGS				
SHEET TITLE	SHEET NUMBER	TITLE	ISSUE DATE	LAST REVISED
SP-1.1	1 OF 3	COVER SHEET W/EXISTING CONDITIONS	OCTOBER 5, 2023	APRIL 17, 2024
SP-2.1	2 OF 3	PROPOSED CONDITIONS	OCTOBER 5, 2023	APRIL 17, 2024
DT-3.1	3 OF 3	CONSTRUCTION DETAILS & NOTES	OCTOBER 5, 2023	APRIL 17, 2024



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20 0 10 20
SCALE: 1" = 20 FT.

APPLICANT

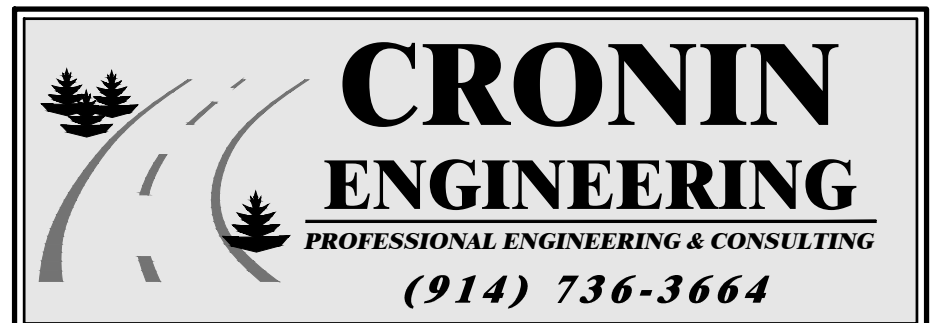
GREENBURGH HEBREW CENTER
515 BROADWAY
DOBBS FERRY, NEW YORK 10522

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REVISIONS

#	REASON	DATE
2	REVISION PER PLANNER & ENGINEER MEMOS	04-17-2024
1	REVISION FOR PLANNING BOARD SUBMISSION	01-22-2024

MUNICIPAL TAX IDENTIFICATION:		
SECTION:	3.40	
BLOCK:	4	
LOT:	11	
SUBLOT:	----	
DRAWN BY:	KJW	
CHECKED:	JCA/PMB	PATRICK M. BELL, P.E. LICENSE #087679
PROJECT:	GREENBURGH HEBREW CTR	
DATE:	OCTOBER 5, 2023	
JOB #:	230702	



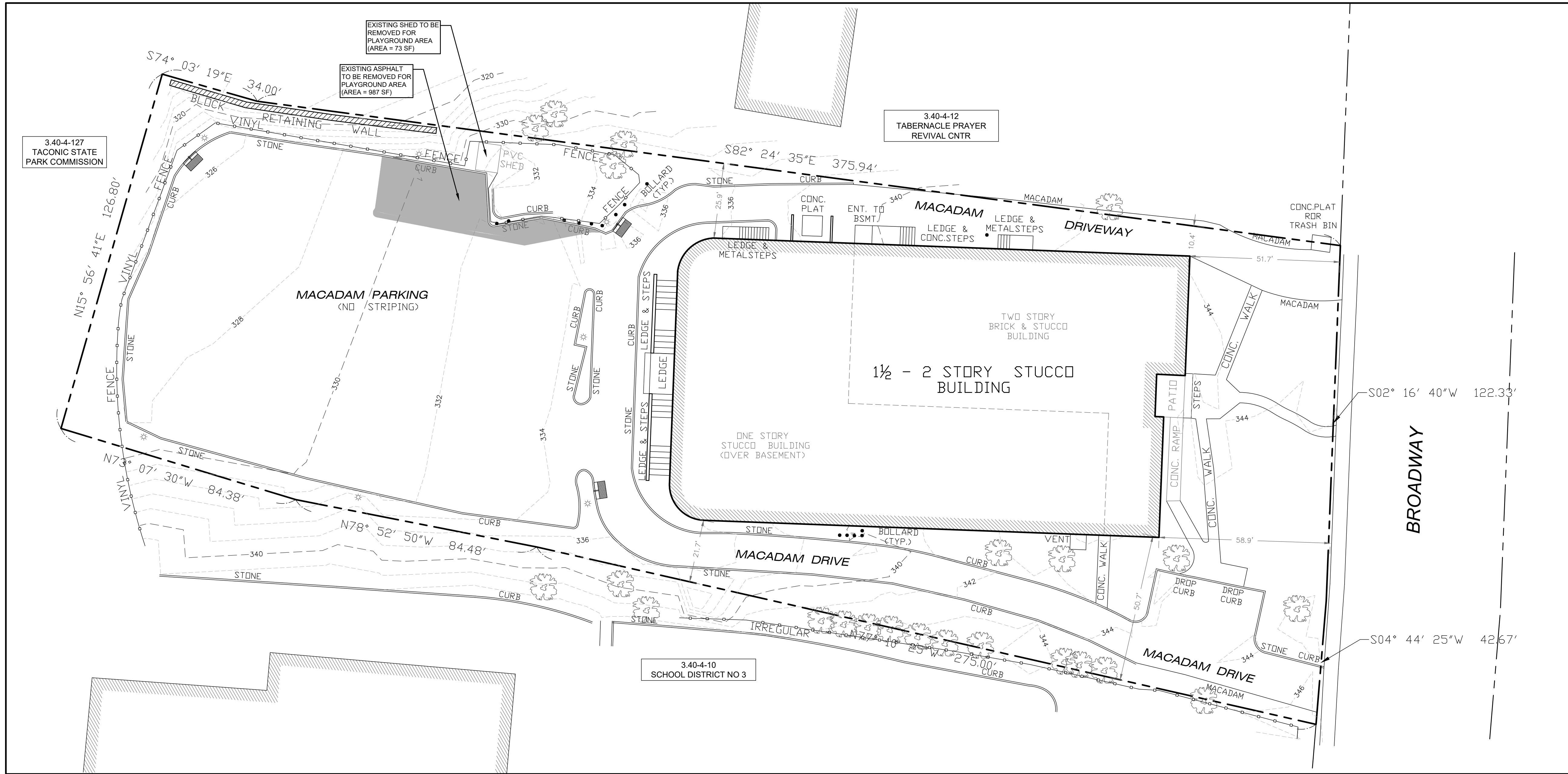
39 Arlo Lane
Cortlandt Manor, New York 10567

EXISTING CONDITIONS

SITE PLAN
FOR
GREENBURGH HEBREW
CENTER

LOCATION:
515 BROADWAY
DOBBS FERRY, NEW YORK

SHEET 1 OF 3 SP-1.1



EXISTING CONDITIONS
SCALE: 1" = 20'

ZONING DATA - ZONE OF-1

SINGLE FAMILY RESIDENCE											
DESCRIPTION	LOT AREA (SF) (MINIMUM)	LOT WIDTH (FT) (MINIMUM)	LOT DEPTH (FT) (MINIMUM)	BLDG COVERAGE (SF) (MAXIMUM)	IMPERVIOUS COVERAGE (SF) (MAXIMUM)	FRONT YARD (FT) (MINIMUM)	REAR YARD (FT) (MINIMUM)	SIDE YARD (EACH) (FT) (MINIMUM)	SIDE YARD (BOTH) (FT) (MINIMUM)	MAXIMUM STORIES	MAXIMUM HEIGHT
REQUIREMENT	40,000	150	150	15% 9,262 SF	30% 18,524 SF	40	40	20	50	2.5	30
EXISTING	61,747	162	428	27% 16,883 SF	75%* 46,096 SF	51.7	189.18	10.4*	32.1*	--	--
PROPOSED	N/C	N/C	N/C	27% 16,590 SF	74% 45,713 SF	N/C	N/C	N/C	N/C	N/C	N/C

N/C = NO CHANGE
* PRE-EXISTING NON-CONFORMING

ENGINEERS NOTES TO OWNER / CONTRACTOR

1. A PRE-CONSTRUCTION SITE INSPECTION WITH CRONIN ENGINEERING, P.E., P.C. IS REQUIRED W/ THE OWNER AND CONTRACTOR PRESENT TO CONFIRM THE CONSTRUCTION PROCEDURE.
2. THERE SHALL BE NO MODIFICATION TO ANY ASPECT OF THIS PLAN WITHOUT FIRST CONTACTING CRONIN ENGINEERING, P.E., P.C. FOR APPROVAL.
3. PRIOR TO ANY EXCAVATION, THE OWNER AND/OR CONTRACTOR SHALL CALL THE UNDERGROUND LINE LOCATION SERVICE (CODE 53) AT (800)-962-7962. FOR MORE INFORMATION, VISIT WWW.DIGSAFELYNEWYORK.COM.
4. EROSION & SEDIMENT CONTROL MEASURES AS SHOWN IN THIS PLAN SET SHALL BE IN PLACE PRIOR TO THE COMMENCEMENT OF ANY SITE WORK. IF UNFORESEEN FIELD CONDITIONS ARE ENCOUNTERED WHICH PROHIBIT THE INSTALLATION OF CERTAIN EROSION & SEDIMENT CONTROL MEASURES AS SHOWN, IT SHALL BE THE OWNER AND/OR CONTRACTOR'S RESPONSIBILITY TO CONTACT CRONIN ENGINEERING, P.E., P.C. IMMEDIATELY TO DISCUSS ALTERNATIVE METHODS. IT SHALL BE THE OWNER AND/OR CONTRACTOR'S RESPONSIBILITY TO ENSURE THE INTEGRITY OF ALL EROSION & SEDIMENT CONTROL MEASURES AT ALL TIMES THROUGHOUT THE DURATION OF THE PROJECT.
5. CRONIN ENGINEERING, P.E., P.C. MAKES NO REPRESENTATIONS OR CERTIFICATIONS AS TO THE INTEGRITY, LOCATION OR EXISTENCE OF SUBSURFACE STRUCTURES OR SOIL CONDITIONS WITH RESPECT TO STABILITY AND SUITABILITY FOR THE INTENDED PURPOSE. IT IS THE RESPONSIBILITY OF THE OWNER AND/OR CONTRACTOR TO VERIFY ALL SUBSURFACE CONDITIONS AND ENSURE THAT ALL IMPROVEMENTS ARE PLACED ON MATERIAL WITH A SUITABLE BEARING CAPACITY.
6. CRONIN ENGINEERING, P.E., P.C. MAKES NO REPRESENTATION OR CERTIFICATIONS AS TO THE QUANTITY OF MATERIAL NEEDED OR TO BE REMOVED FOR THE SUCCESSFUL CONSTRUCTION OF THE PROJECT. IT IS THE RESPONSIBILITY OF THE OWNER AND/OR CONTRACTOR TO VERIFY THE QUANTITY OF MATERIAL NEEDED OR TO BE REMOVED TO SUCCESSFULLY CONSTRUCT THE PROJECT.
7. IN THE EVENT THAT FIELD CONDITIONS ARE DIFFERENT THAN WHAT IS PRESENTED IN THIS PLAN SET, IT IS THE RESPONSIBILITY OF THE OWNER AND/OR CONTRACTOR TO NOTIFY CRONIN ENGINEERING, P.E., P.C. PRIOR TO CONTINUING WITH ANY FURTHER SITE WORK.
8. IF UNFORESEEN SUBSURFACE CONDITIONS ARE ENCOUNTERED (I.E. ROCK, GROUNDWATER, ETC.), THE OWNER AND/OR CONTRACTOR SHALL STOP WORK AND NOTIFY CRONIN ENGINEERING, P.E., P.C. ALL NECESSARY MODIFICATIONS OR CHANGES SHALL BE DISCUSSED WITH AND APPROVED BY CRONIN ENGINEERING, P.E., P.C. PRIOR TO CONTINUING WITH ANY FURTHER SITE WORK. FURTHERMORE, THE OWNER AND/OR CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS IF BLASTING IS REQUIRED.
9. IT IS THE OWNER AND/OR CONTRACTOR'S SOLE RESPONSIBILITY TO FOLLOW OSHA, NYS AND ANY OTHER APPLICABLE CODES OR REQUIREMENTS THROUGHOUT THE DURATION OF THE PROJECT. THIS INCLUDES BUT IS NOT LIMITED TO PROVIDING ADEQUATE BRACING AND GUARANTEEING THE STABILITY OF EXCAVATIONS AND OTHER VICINITY STRUCTURES.
10. CRONIN ENGINEERING, P.E., P.C. MAKES NO REPRESENTATION AS TO THE QUALITY (I.E. CONTAMINATION), IF ANY, OF THE SOILS ON THIS SITE. THE OWNER AND/OR CONTRACTOR ARE RESPONSIBLE TO CONDUCT ANY AND ALL TESTING AS MAY BE REQUIRED TO ENSURE THE SITE HAS NO CONTAMINATED SOILS.
11. CRONIN ENGINEERING, P.E., P.C. MAKES NO REPRESENTATION AS TO THE SOIL BEARING CAPACITY OF THE SOILS ON SITE. THE OWNER AND/OR ARCHITECT ARE RESPONSIBLE TO CONDUCT ANY AND ALL TESTING AS MAY BE REQUIRED TO ENSURE THE CONSTRUCTION OF THE FOUNDATION MEETS THE REQUIREMENTS OF THE NEW YORK STATE BUILDING CODE AND THE BUILDING CODE OF THE VILLAGE OF BUCHANAN. FOUNDATION DESIGN IS NOT PROVIDED BY THIS OFFICE AND IS THE RESPONSIBILITY OF THE OWNER/ARCHITECT/BUILDER.
12. THE OWNER/BUILDER/ARCHITECT ARE RESPONSIBLE TO ENSURE THAT THE BUILDING FOUNDATION MEETS ALL VILLAGE CODES AND THE NEW YORK STATE RESIDENTIAL BUILDING CODE. IF UNSUITABLE MATERIAL IS ENCOUNTERED, THE OWNER SHALL MAKE PROVISIONS (I.E. DEEPER FOOTING, SPREAD FOOTING, GRADE BEAMS, PILES, ETC.) AS NECESSARY TO ENSURE A CODE COMPLIANT FOUNDATION TO THE SATISFACTION OF THE VILLAGE OF DOBBS FERRY BUILDING DEPARTMENT.

SPECIAL NOTE

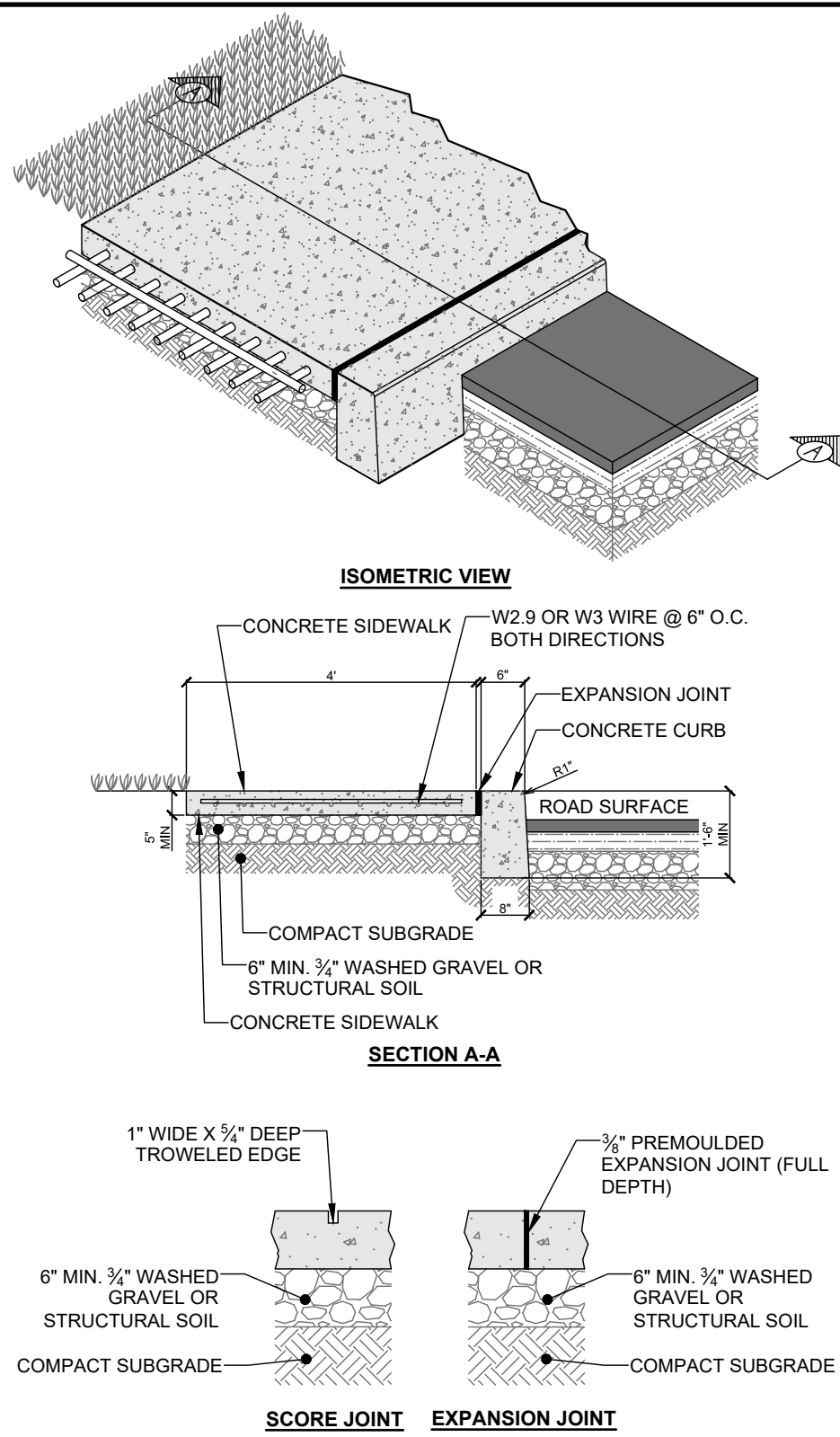
THE DESIGN ENGINEER ASSUMES NO RESPONSIBILITY FOR THE MEANS AND METHODS NEEDED TO PERFORM THE CONSTRUCTION/EXCAVATION, ET AL, OPERATIONS SHOWN HEREON. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE A SAFE WORKING ENVIRONMENT AND PROVIDE THE MEANS AND METHODS TO PERFORM THE NECESSARY TASKS. NOTHING ON THESE PLANS SHALL OBLIGATE THE DESIGN ENGINEER AS TO THE MEANS AND METHODS TO PERFORM THE TASKS ASSOCIATED WITH THE APPROVED DESIGN PLANS. THOSE OBLIGATIONS AT ALL TIMES REMAIN WITH THE CONTRACTOR(S).

EROSION AND SEDIMENT CONTROL NOTES

1. CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLIANCE WITH ALL SEDIMENT AND EROSION CONTROL PRACTICES. THE SEDIMENT AND EROSION CONTROL PRACTICES ARE TO BE INSTALLED PRIOR TO ANY MAJOR SOIL DISTURBANCES, AND MAINTAINED UNTIL PERMANENT PROTECTION IS ESTABLISHED.
2. TIMELY MAINTENANCE OF SEDIMENT CONTROL STRUCTURES IS THE RESPONSIBILITY OF THE CONTRACTOR. ALL STRUCTURES SHALL BE MAINTAINED IN GOOD WORKING ORDER AT ALL TIMES. THE SEDIMENT LEVEL IN ALL SEDIMENT TRAPS SHALL BE CLOSELY MONITORED AND SEDIMENT REMOVED PROMPTLY WHEN MAXIMUM LEVELS ARE REACHED OR AS ORDERED BY THE ENGINEER. ALL SEDIMENT CONTROL STRUCTURES SHALL BE INSPECTED WEEKLY, PRIOR TO EXPECTED RAIN EVENTS, AND AFTER EACH HEAVY RAIN TO ENSURE PROPER OPERATION AS DESIGNED. AN INSPECTION SCHEDULE SHALL BE SET FORTH PRIOR TO THE START OF CONSTRUCTION.
3. THE LOCATIONS AND THE INSTALLATION TIMES OF THE SEDIMENT CAPTURING STANDARDS SHALL BE AS ORDERED BY THE ENGINEER, AND IN ACCORDANCE WITH ACCEPTED STANDARDS.
4. ALL TOPSOIL NOT TO BE USED FOR FINAL GRADING SHALL BE REMOVED FROM THE SITE IMMEDIATELY AND PLACED IN A STABILIZED STOCKPILE OR FILL AREA. ALL TOPSOIL REQUIRED FOR FINAL GRADING AND STORED ON SITE SHALL BE LIMED, FERTILIZED, TEMPORARILY SEEDED AND MULCHED WITHIN 14 DAYS.
5. ANY DISTURBED AREAS THAT WILL BE LEFT EXPOSED MORE THAN 21 DAYS AND NOT SUBJECT TO CONSTRUCTION TRAFFIC, SHALL IMMEDIATELY RECEIVE TEMPORARY SEEDING. MULCH SHALL BE USED IF THE SEASON PREVENTS THE ESTABLISHMENT OF A TEMPORARY COVER. DISTURBED AREAS SHALL BE LIMED AND FERTILIZED PRIOR TO TEMPORARY SEEDING.
6. ALL DISTURBED AREAS WITHIN 500 FEET OF AN INHABITED DWELLING SHALL BE WETTED AS NECESSARY TO PROVIDE DUST CONTROL.
7. THE CONTRACTOR SHALL KEEP THE ROADWAYS WITHIN THE PROJECT CLEAR OF SOIL AND DEBRIS AND IS RESPONSIBLE FOR ANY STREET CLEANING NECESSARY DURING THE COURSE OF THE PROJECT.
8. SEDIMENT AND EROSION CONTROL STRUCTURES SHALL BE REMOVED AND THE AREA STABILIZED WHEN THE DRAINAGE AREA HAS BEEN PROPERLY STABILIZED BY PERMANENT MEASURES.
9. SOIL SEEDING AND FERTILIZER AMENDMENTS SHALL BE PERFORMED IN ACCORDANCE WITH THE CURRENT EDITION OF "NEW YORK GUIDELINES FOR URBAN EROSION AND SEDIMENT CONTROL".
10. ALL SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH THE CURRENT EDITION OF "NEW YORK GUIDELINES FOR URBAN EROSION AND SEDIMENT CONTROL".

VILLAGE NOTES

1. SHOULD ROCK BLASTING BE REQUIRED, A PERMIT APPLICATION IN ACCORDANCE WITH CHAPTER 125 - BLASTING AND EXPLOSIVES OF THE VILLAGE OF DOBBS FERRY CODE MUST BE SUBMITTED TO THE VILLAGE BY THE APPLICANT FOR REVIEW / APPROVAL.
2. THE VILLAGE ENGINEER MAY REQUIRE ADDITIONAL EROSION CONTROL MEASURES IF DEEMED APPROPRIATE TO MITIGATE UNFORESEEN SILTATION AND EROSION OF DISTURBED SOILS.
3. BUILT PLANS OF THE PROPOSED DRIVEWAY AND DRAINAGE IMPROVEMENTS SHALL BE SUBMITTED TO THE VILLAGE ENGINEER FOR REVIEW PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY.
4. FILL MATERIAL IMPORTED TO THE SITE SHALL BE CERTIFIED IN WRITING BY A NEW YORK LICENSED PROFESSIONAL ENGINEER AS CLEAN, NON-CONTAMINATED FILL SUITABLE FOR THE INTENDED USE.
5. BEFORE THE SITE PLAN IS SIGNED BY THE CHAIRMAN OF THE PLANNING BOARD, THE APPLICANT SHALL BE REQUIRED TO POST A PERFORMANCE BOND OR OTHER TYPE OF ACCEPTABLE MONETARY GUARANTY WHICH SHALL BE IN AN AMOUNT DETERMINED BY THE PLANNING BOARD AND THE VILLAGE ENGINEER AND IN A FORM SATISFACTORY TO THE VILLAGE ATTORNEY.
6. THE APPLICANT SHALL NOTIFY THE BUILDING DEPARTMENT VILLAGE'S CONSULTING ENGINEER IN WRITING AT LEAST 48 HOURS BEFORE ANY OF THE FOLLOWING SO THAT ANY INSPECTION MAY BE PERFORMED:
 - START OF CONSTRUCTION
 - INSTALLATION OF SEDIMENT AND EROSION CONTROL MEASURES
 - COMPLETION OF SITE CLEARING
 - COMPLETION OF ROUGH GRADING
 - INSTALLATION OF SMP'S
 - COMPLETION OF FINAL GRADING AND STABILIZATION OF DISTURBED AREAS
 - CLOSURE OF CONSTRUCTION
 - COMPLETION OF FINAL LANDSCAPING
 - SUCCESSFUL ESTABLISHMENT OF LANDSCAPING IN PUBLIC AREAS
7. THE OWNER OR OPERATOR SHALL HAVE A QUALIFIED INSPECTOR INSPECT AND DOCUMENT THE EFFECTIVENESS OF ALL EROSION AND SEDIMENTATION CONTROL PRACTICES AND PREPARE INSPECTION REPORTS AT LEAST ONCE A MONTH. THESE REPORTS MUST BE KEPT ON SITE AND AVAILABLE FOR REVIEW.
8. THE PAVED STREET ADJACENT TO THE SITE ENTRANCE SHALL REMAIN CLEAR AND WILL BE SWEEP DAILY TO REMOVE ANY EXCESS DIRT, MUD OR ROCK TRACKED OFFSITE.



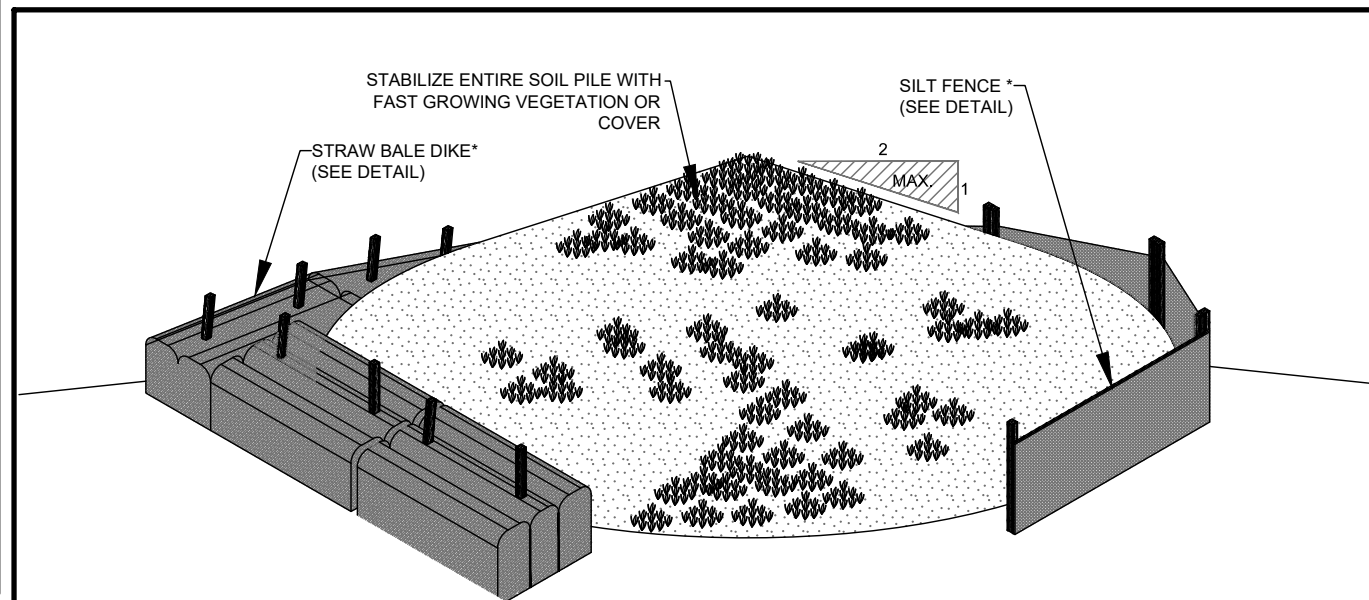
CONSTRUCTION NOTES:

1. CONTRACTOR SHALL COMPLY WITH REQUIREMENTS OF SECTION 501 NYS DOT STANDARD SPECIFICATIONS CODE FOR TRANSPORTING AND PLACING OF PORTLAND CEMENT CONCRETE.
2. CONCRETE TO BE PLACED IN ONE COURSE TO THE FULL DEPTH.
3. PROVIDE CONCRETE CLASS "A" AS PER NYS DOT SECTION 501.2.
4. CURING OF CONCRETE SHALL CONFORM SECTION 502 NYS CODE.
5. SIDEWALK TO BE PLACED ON 6" THICK BASE OF COMPACTED 3/4" GRAVEL OR STRUCTURAL SOIL (AS SPECIFIED).
6. SIDEWALK SLAB SHALL HAVE A MINIMUM THICKNESS OF 5" AND 7" MIN. AT DRIVEWAY SECTIONS.
7. WIRE FABRIC TO BE W2.9 OR W3 WIRE AT 6" CENTERS TRANSVERSELY AND LONGITUDINALLY, AND SHALL BE EMBEDDED TO MID-DEPTH IN THE SLAB.
8. LONGITUDINAL JOINTS TO BE PLACED TO FULL DEPTH OF SLAB BETWEEN SIDEWALK AND CURB.
9. TRANSVERSE EXPANSION JOINTS SHALL BE PLACED TO THE FULL DEPTH OF SLAB AND BE SPACED 20 FEET APART. THE EDGES OF TRANSVERSE JOINTS TO BE FINISHED WITH AN EDGING TOOL HAVING A 1/4 INCH RADIUS.
10. ALL JOINTS SHALL BE PRE-MOLDED BITUMINOUS TYPE, 3/4" OR 1/2".
11. CONCRETE SURFACE TO BE SCORED AND TOOLED AT INTERVALS OF 5 FEET.
12. THE CONCRETE SHALL BE FINISHED TO PRODUCE A SMOOTH SURFACE, THEN LIGHTLY BROOMED TO A UNIFORM TEXTURE, AND SHOULD BE SLIP RESISTANT.
13. CONTRACTOR TO PREVENT AGAINST ANY LOW SPOTS WHERE WATER CAN COLLECT AND ANY POSSIBLE REDIRECTION OF STORMWATER.
14. CONTRACTOR TO USE RESILIENT JOINT FILLER ACCORDING TO SECTION 705-07 NYS CODE AND INSTALL AT ALL JOINTS BETWEEN SIDEWALK, RAMPS, AND CURBS.
15. CONCRETE SIDEWALK TO RECEIVE (2) TWO COATS OF ANTI-SPALLING COMPOUND.

CONCRETE SIDEWALK INSTALLATION N.T.S.



CONCRETE WASHOUT BIN

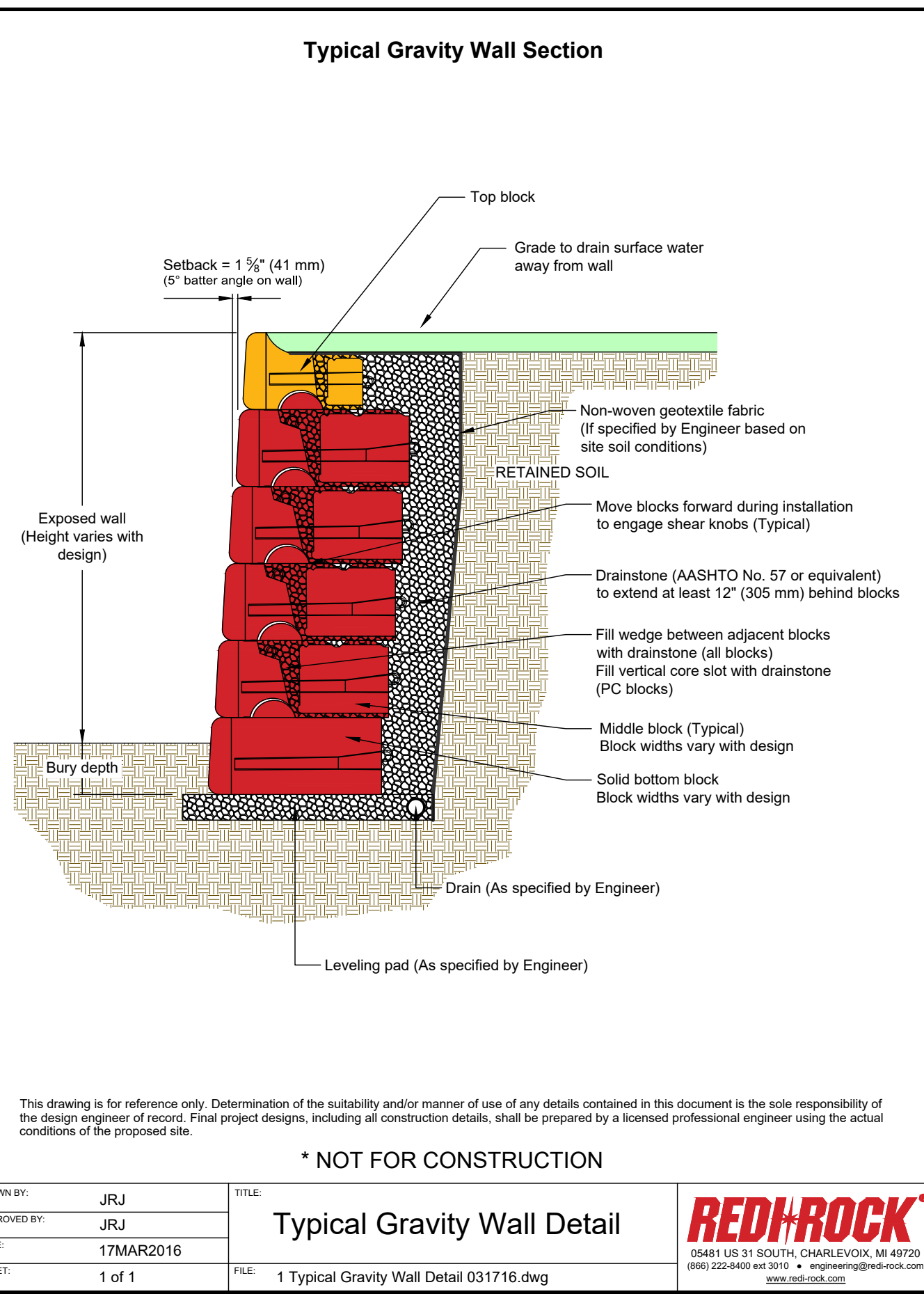


INSTALLATION NOTES:

1. AREA CHOSEN FOR STOCK PILING OPERATIONS TO BE DRY AND STABLE.
2. MAXIMUM SLOPE OF PILE TO BE 1 VERTICAL ON 2 HORIZONTAL.
3. REFER TO STRAW BALE DIKE AND SILT FENCE INSTALLATION DETAILS IN THIS PLAN SET FOR INSTALLATION GUIDELINES.

* EITHER A STRAW BALE DIKE OR SILT FENCE MAY BE USED FOR STOCKPILE PROTECTION AS SPECIFIED BY QUALIFIED INSPECTOR.

SOIL STOCKPILING CONSTRUCTION N.T.S.



* NOT FOR CONSTRUCTION

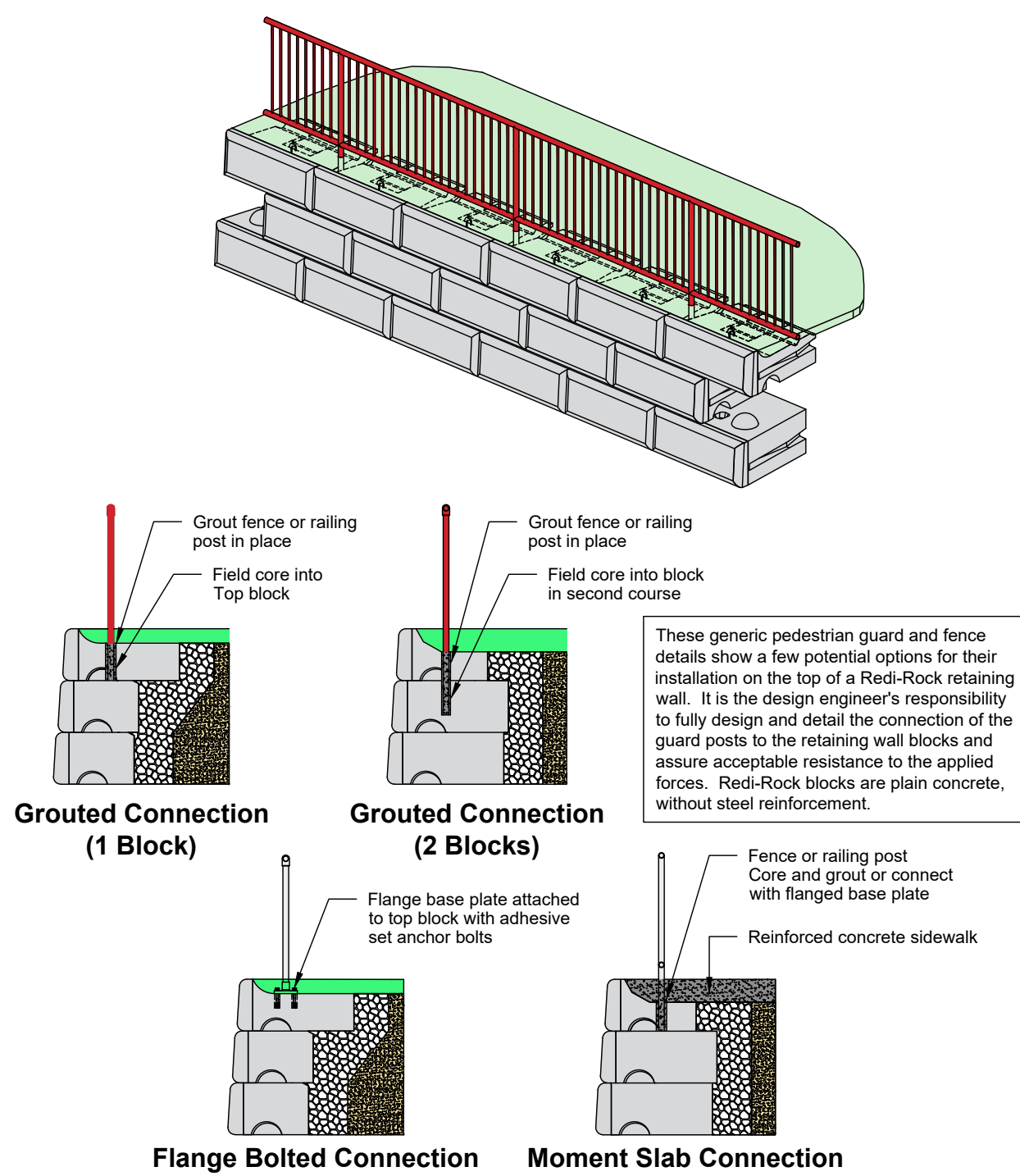
Typical Gravity Wall Detail

DRAWN BY: JRJ
APPROVED BY: JRJ
DATE: 17MAR2016
SHEET: 1 of 1

FILE: 1 Typical Gravity Wall Detail 031716.dwg

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Fence or Pedestrian Guard Connection Options



This drawing is for reference only. Determination of the suitability and/or manner of use of any details contained in this document is the sole responsibility of the design engineer of record. Final project designs, including all construction details, shall be prepared by a licensed professional engineer using the actual conditions of the proposed site.

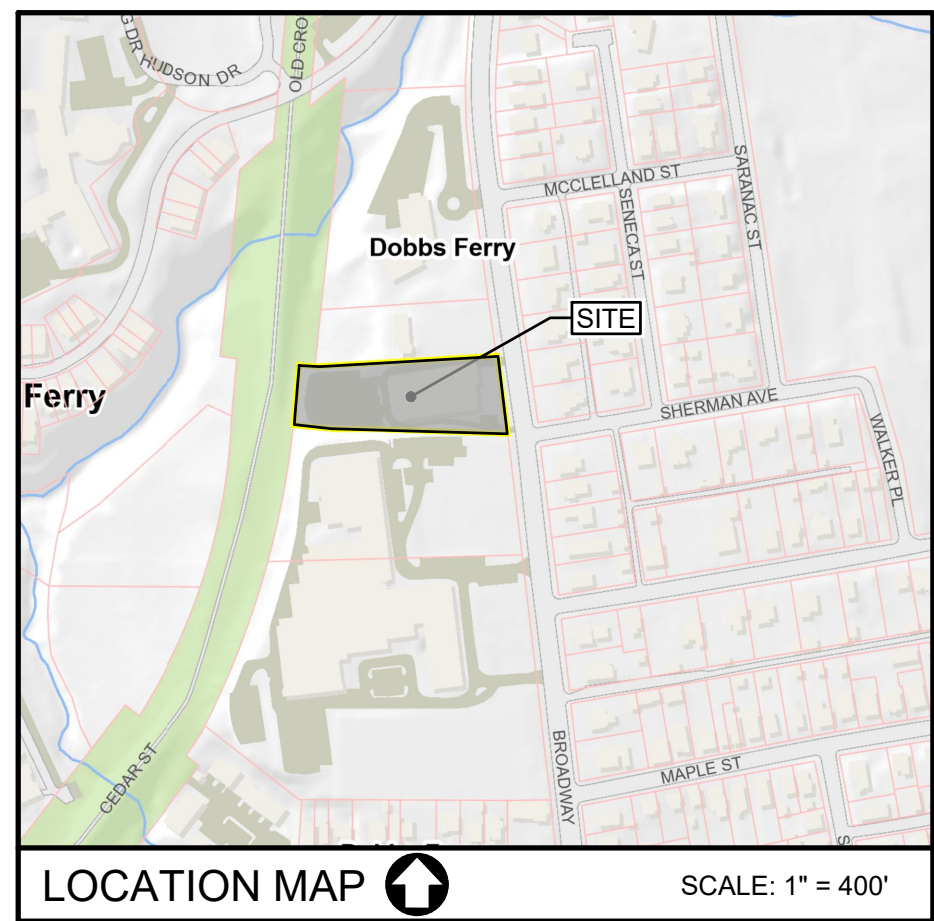
* NOT FOR CONSTRUCTION

Fence or Pedestrian Guard Connection Options

DRAWN BY: JRJ
APPROVED BY: JRJ
DATE: 06-22-2015
SHEET: 1 of 1

FILE: 5 Fence or Pedestrian Guard Connection Options 062215.dwg

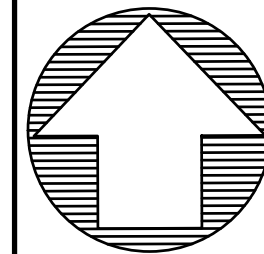
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LOCATION MAP

SCALE: 1" = 400'

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SCALE: 1" = 20 FT.

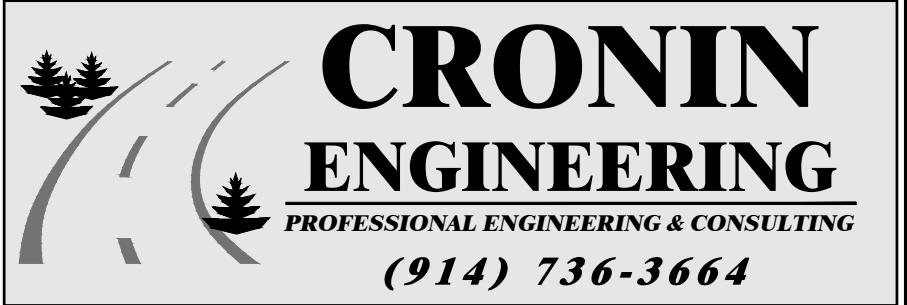
APPLICANT

GREENBURGH HEBREW CENTER
515 BROADWAY
DOBBS FERRY, NEW YORK 10522

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REVISIONS

#	REASON	DATE
2	REVISION PER PLANNER & ENGINEER MEMOS	04-17-2024
1	REVISION FOR PLANNING BOARD SUBMISSION	01-22-2024
MUNICIPAL TAX IDENTIFICATION:		
SECTION:	3.40	
BLOCK:	4	
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DRAWN BY:	KJW	
CHECKED:	JCA/PMB	
PROJECT:	GREENBURGH HEBREW CTR	
DATE:	OCTOBER 5, 2023	
JOB #:	230702	
PATRICK M. BELL, P.E. LICENSE #087679		



39 Arlo Lane
Cortlandt Manor, New York 10567

CONSTRUCTION DETAILS

SITE PLAN FOR GREENBURGH HEBREW CENTER

LOCATION:
515 BROADWAY
DOBBS FERRY, NEW YORK

SHEET 3 OF 3

DT-3.1