



VILLAGE OF DOBBS FERRY BOARD OF TRUSTEES AGENDA

MEETING DATE: JULY 13, 2021
AGENDA ITEM SECTION: PUBLIC HEARINGS
AGENDA ITEM NO. : 1
AGENDA ITEM: CONTINUATION OF PUBLIC HEARING ON THE PROPOSED ZONING CODE AND MAP AMENDMENTS RECOMMENDED BY THE PLANNING BOARD
ITEM BACKUP DOCUMENTATION: 1. INTRODUCTORY LOCAL LAW TO AMEND ZONING CODE 2. CHARACTER GUIDELINES FOR PALISADE STREET

ITEM BACKUP DOCUMENTATION:

1. INTRODUCTORY LOCAL LAW TO AMEND ZONING CODE
2. CHARACTER GUIDELINES FOR PALISADE STREET

INTRODUCTORY LOCAL LAW TO AMEND ZONING CODE

“Amendments to the Village of Dobbs Ferry Zoning Code to Adopt and Incorporate ‘Character Guidelines on Palisade Street’, Clarify Area and Bulk Tables in Appendix B and Revise the Zoning Map to relocate the MDR-1 Boundary to Include both Sides of Maple Street Between South Broadway and Park Road and Re-Zone Single Parcel at Northwest Corner of Rochambeau Avenue and Broadway from MDR-1 to B”

A LOCAL LAW to: (i) adopt Character Guidelines on Palisade Street” and amend the text of Chapter 300 “Zoning and Land Use”, at Section 300-35 to reflect the adoption and append guidelines as Appendix H; (ii) amend the Area and Bulk Tables B-2, B-3, B-6 & B-7 in Appendix B; and (iii) amend the official zoning map to relocate the MDR-1 district boundary to include both sides of Maple Street between South Broadway and Park Road and move parcel located on the northwest corner of Rochambeau Avenue and Broadway from MDR-1 to B Zone.

BE IT ENACTED by the Board of Trustees of the Village of Dobbs Ferry, as follows:

Section 1. Legislative Intent. In furtherance of its overarching goal of promoting the health, safety and welfare of the community, and in support of maintaining property values and encouraging the most appropriate use of land, the Village Board of Trustees has considered zoning amendments recommended by the Planning Board and determined that they are balanced and appropriate to address identified issues, in keeping with the comprehensive vision plan and will achieve three key objectives:(i) reduce pre-existing non-conformities and better align zoning requirements with existing development patterns and practice; (ii) provide guidance to applicants and land use boards to improve design outcomes for future development on Palisade Street between Chestnut and Cedar Streets, an area having uniqueness of character based on existing scale and context; and (iii) clarify area and bulk dimensional tables in Appendix B which in application have proved problematic.

Section 2. “Character Guidelines for Palisades Street” are hereby adopted, shall be labeled with the date of adoption and attached as Appendix H.

Section 3. Section 300-35 (B) of Chapter 300 of the Code of Dobbs Ferry is hereby amended as follows, with new matter underlined and deleted matter ~~struck~~ and in [brackets]:

B. [~~Residential~~] Conformance with Design and Character guidelines.

(1) Conformance with the Residential Design Guidelines, Appendix G, as adopted by the Board of Trustees, is encouraged where applicable and shall be considered as part of the site plan review criteria, as well as the basis for the Architectural and Historic Review Board's evaluation.

(2) Conformance with the Character Guidelines for Palisades Street, Appendix H, as adopted by the Board of Trustees, is encouraged where applicable and shall be considered as part of the site plan review criteria, as well as the basis for the Architectural and Historic Review Board's evaluation.

Section 4. Section 300-35 (5)(c) of Chapter 300 of the Code of Dobbs Ferry is hereby amended as follows, with new matter underlined and deleted matter struck and in [brackets]:

For a building in the MDR-2 which has a building width greater than 40 feet as measured parallel to Palisade Street, the portion of the building width that is greater than 40 feet shall have an additional 5 foot front yard setback.

Section 4. Appendix B to Chapter 300 of the Code of the Village of Dobbs Ferry, Tables B-1, B-2, B-3, B-6 & B-7 are hereby amended as follows:

Table B-1: OF Districts Dimensional Standards

Zone	OF-6	OF-5	OF-4	OF-3	OF-2	OF-1
Minimum net lot area (square feet)	5,000	7,500	10,000	15,000	20,000	40,000
Minimum lot width (feet)	50	75	100	100	125	150
Minimum lot depth (feet)	100	100	100	100	125	150
Maximum lot coverage by buildings	27%	25%	22%	20%	18%	15%
Maximum lot coverage by impervious surfaces	54%	44%	40%	40%	40%	30%
Minimum front yard setback (feet)	20	25	25	30	30	40
Maximum front yard setback (feet)	prevailing	prevailing	prevailing	NA	NA	NA
Minimum rear yard setback	25	25	25	25	25	40
Minimum side yard setback (each) (feet)	10	10	12	15	20	20

Minimum side yard setback(both) (feet)	20	25	30	40	50	50
<u>Maximum Stories</u>	<u>2 ½</u>					
<u>Maximum Height</u>	<u>As per Table B-6 and B-7</u>					

Note:

The net lot area is determined by deducting the adjustments specified in § 300-34A(2) from gross lot area.

Table B-2: MDR, B and MF Districts Dimensional Standards

Zone	MDR-	MDR-2	MDR-H	B	MF-1	MF-2	MF-3	MF-4
Minimum net lot area (square foot)	5,000	5,000	10,000	5,000	20,000	20,000	20,000	20,000
Minimum lot area per dwelling unit (square foot)	2,500	800	2,500	800	6,000	3,500	2,500	1,750
Minimum lot width (feet)	50	50	50	50	100	100	100	100
Minimum lot depth (feet)	100	100	100	100	100	100	100	200
Maximum lot coverage by buildings	27%	[27%] 40%	30%	30%	30%	30%	40%	40%
Maximum lot coverage by impervious surfaces	54%	[54%] 60%	60%	60%	60%	60%	60%	60%
Minimum front yard setback (feet)	20	[20] 15	20	prevailing	25	25	50	10
Minimum rear yard setback (feet)	25	25	25	25	30	30	30	30
Minimum side yard setback (each) (feet)	10	10	10	10	25	25	25	10
Minimum side yard setback (both) (feet)	20	20	20	20	50	50	50	20
<u>Maximum Stories</u>	<u>2.5</u>	<u>3^(a)</u>	<u>3</u>	<u>3</u>	<u>3</u>	<u>3</u>	<u>4</u>	<u>3</u>
<u>Maximum height</u>	<u>As per Table B-6</u>							

Note:

The net lot area is determined by deducting the adjustments stipulated in § 300-34A(2) from gross lot area.

Abbreviations:

*GP = Grade plane

*R = Ridge

*MP = Midpoint of sloped roof

*TF = Top of flat roof

*C = Curb

(a) 3rd ~~Floor~~ Story must be set back 5 feet as measured from the front of the building at ground level. For buildings with a building width greater than 40 feet, the 5 foot setback shall be measured from the front of the building at ground level within the first 40 feet of building width.

Table B-3: Residential Lot Dimensions and Coverage (Sliding Scales)¹

Lot area	Less Than 7,500 Square Feet	At Least 7,500 Square Feet but Less Than 10,000	At Least 10,000 Square Feet but Less Than 20,000 Square Feet	At Least 20,000 Square Feet but Less Than 40,000 Square Feet	40,000 Square Feet or Greater
Minimum lot width (feet)	Lot area/100	Lot area/100	100	100	150
Minimum lot depth (feet)	100	100	100	125	150
Maximum lot coverage by buildings	{27%} As per Tables B1 & B2	{22%} x .82	{20%} x .75	{18%} x .67	{15%} x .56
Maximum lot coverage by impervious surfaces	{54%} As per Tables B1 & B2	{44%} x .82	{40%} x .75	{40%} x .67	{30%} x .56

Note: ¹ Multiplier to be applied to maximum lot coverage by buildings as per Tables B1 or B2

Table B-6: Residential Building Height (Sliding Scales)¹

Roof Pitch (Rise/Run)	0 to 0.25	0.25 to 0.30	Greater than 0.30	Maximum Height to Eave
OF [and] MDR-1 <u>and GP-R</u> Districts (feet)	30	33	35	28
[MDR-2, B,] MF-1 MF-2 and <u>GP-MP/TF</u> Districts (feet)	30	35	40	28
<u>MF-3 District (feet)</u>	<u>40</u>	<u>N/A</u>	<u>N/A</u>	<u>N/A</u>
{MF-3 and} MF-4 District[s] (feet)	35	40	42	N/A
<u>MDR-2 and C-TF/MP Districts (feet)</u>	<u>37</u>	<u>40</u>	<u>42</u>	<u>N/A</u>
<u>B and C-TF/MP</u>	<u>40</u>	<u>43</u>	<u>45</u>	<u>N/A</u>

NOTES: ¹ Reference § 300-35D(8)(a)[1]-[2].

² Multiplier to be applied to maximum lot coverage by buildings as per Table B1 or B2

**Table B-7: Residential Context Based Height¹ and Massing Limits²
For OFs and MDR-1**

Requirement	Set Limits	Context Based Limits
Maximum ridge height	Default [28] <u>30</u>	Maximum 1.25 times the average ridge height of buildings on lots within the context limit area subject property.
Maximum eave height	Default [22] <u>28</u>	Maximum 1.15 times the average eave height of buildings on lots within the context limit area subject property.
Sky exposure plane <u>OF & MDR-1</u>		A plane set <u>at</u> an [a] angle of 45° projected into the lot from a point set 10 feet above a lot line with the base of the 10 feet set at the average grade height between the lot line and the building.

NOTE: ¹ Every roof must comply with all building height requirements.
² Reference § 300-35D(8)

Abbreviations:

*GP = Grade plane

*R = Ridge

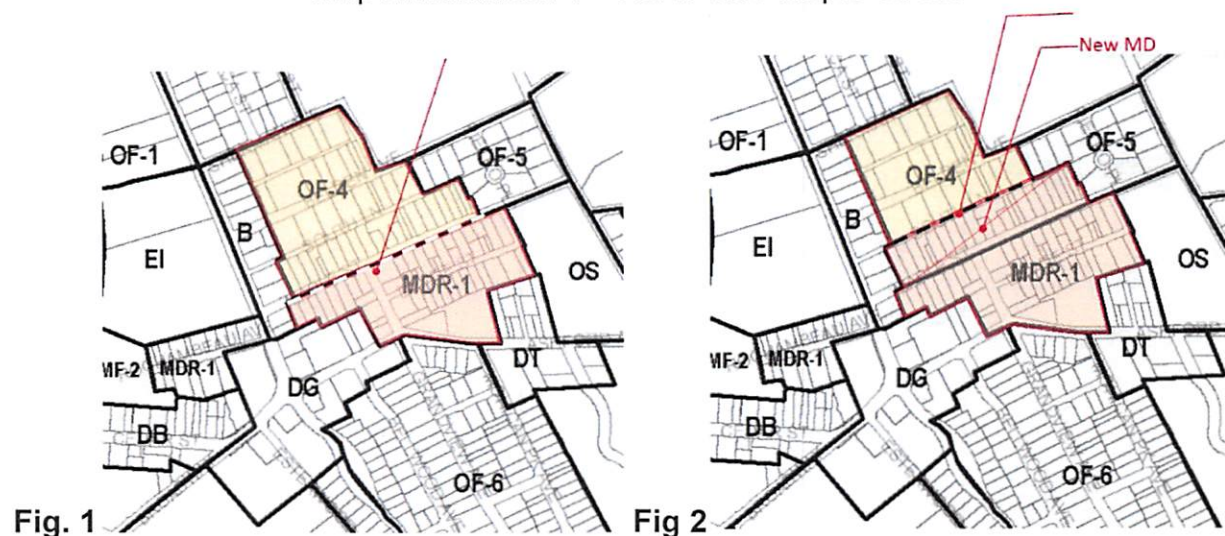
*MP = Midpoint of sloped roof

*TF = Top of flat roof

*C = Curb

Section 5. The Official Map of the Village of Dobbs Ferry is hereby amended by two (2) changes in zoning district boundaries as follows:

Map Amendment 1 – North Side Maple Street



Existing Boundary between MDR-1 and OF-4 in center of Maple Street (fig 1) to be shifted north resulting in properties on both sides of Maple Street having the same MDR-1 Zoning classification (fig 2).

Map Amendment 2 – northwest corner Rochambeau Avenue and Broadway

Fig 3 Existing Boundary

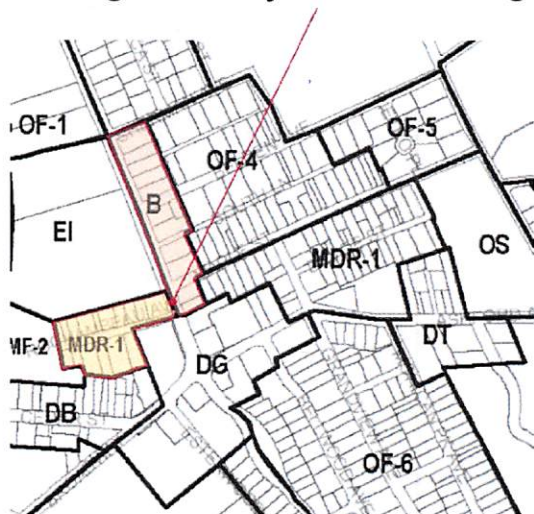
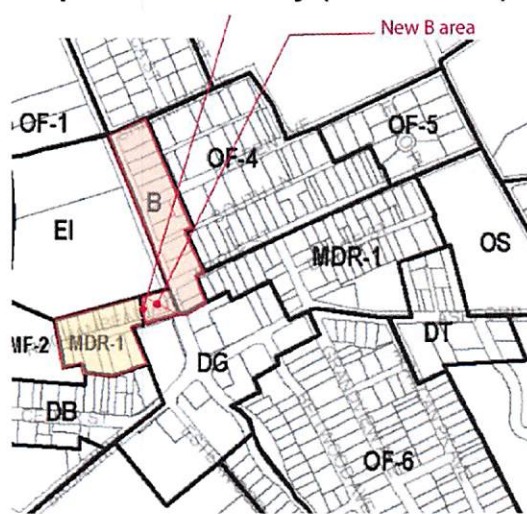


Fig 4 Proposed Boundary (new B area)



Existing Boundary in center of Broadway between MDR-1 and B zones (fig 3) to be shifted to move one large parcel west of Broadway into the B zone (fig 4).

Section 6. Except as otherwise provided herein, all other provisions of Chapter 300 shall remain the same.

Section 5. This Local Law shall take effect immediately upon filing with Secretary of State and shall be published and posted as required by NYS Village Law.

CHARACTER GUIDELINES FOR PALISADE STREET

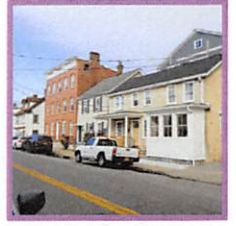




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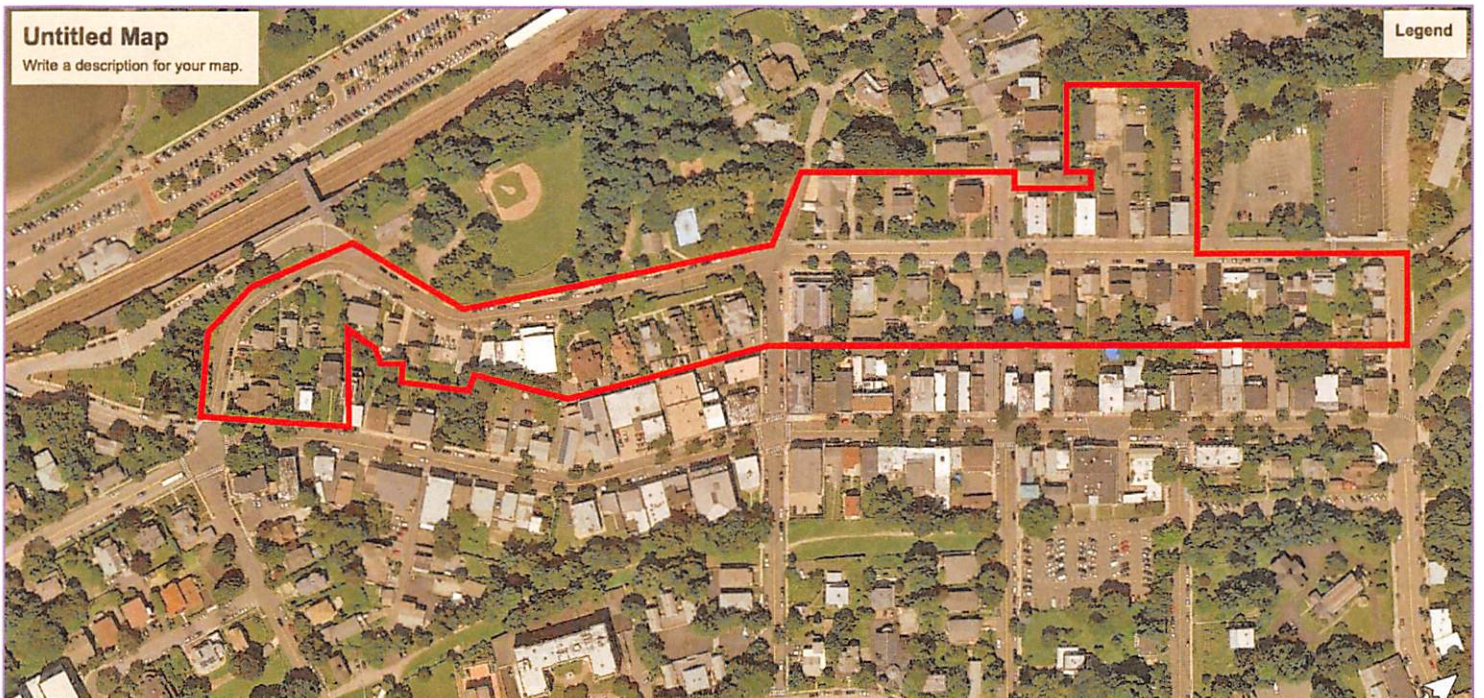
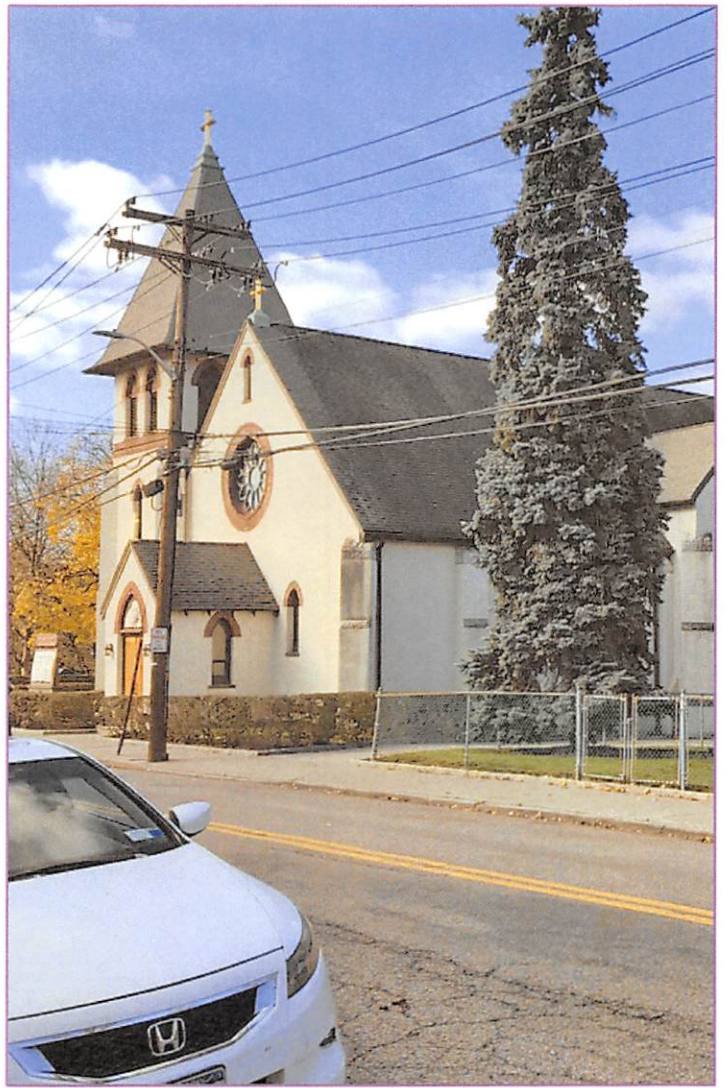
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Aerial view looking east of Palisade Street between Cedar Street and Chestnut Street. The red line represents the boundary of MDR-2 District. This is the focus area for these character guidelines.



Current aerial photo of downtown Dobbs Ferry. The red line represents the boundary of the MDR-2 district.

Palisade Street

Palisade Street Character Guidelines

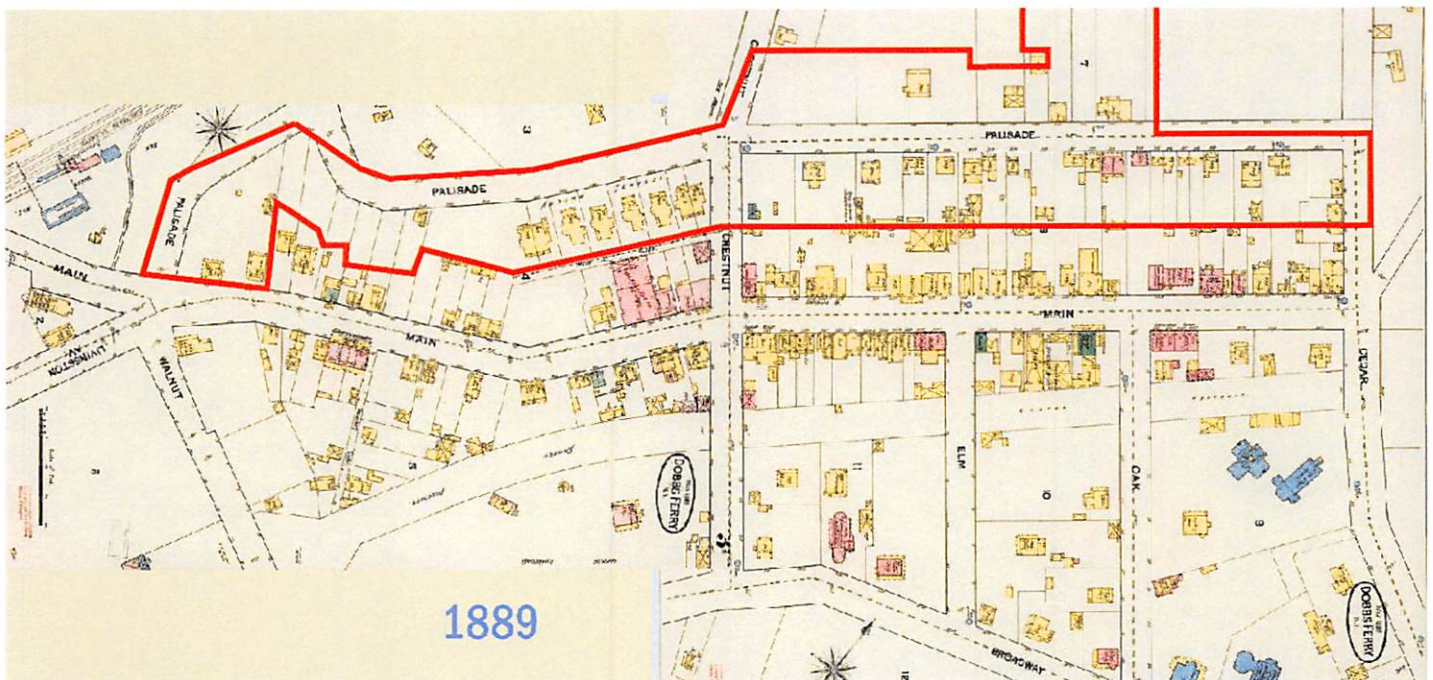
Palisade Street is part of Dobbs Ferry's historic downtown. A comparison below of the 1889 historic map and a current aerial photo shows that most of the buildings on the street today were extant in 1889. Of particular note is Hudson Terrace at the south end of the zone. This beautiful row of single-family houses sitting on a bluff overlooking the Hudson River is completely intact. Although it is an important part of the overall character of Palisade Avenue, it is not the focus of these design guidelines because any new development there would need to conform closely to what is already a very strong context of scale and architectural language. Here, the project review process is more directly informed by the convention that new construction "shall not be excessively similar or dissimilar" to the existing context, which is very strong in this location. The pending Downtown Historic District Guidelines are also a resource for applications in this part of the Downtown.

The context is not as strong along the rest of Palisade Avenue between Chestnut Street and Cedar Street. While, as noted above, most of the houses here are of similar age and character, there are many disparate conditions.

- there are very small houses on small lots
- there are 3-story apartment buildings
- there are former industrial buildings

The east side is more uniform and consistent in terms of the relationship of the buildings to the sidewalk. The west side has major discontinuities in the frontage conditions with some large setbacks and oversized buildings as well as large undeveloped sites at the north end.

Never the less, the street as a whole does have a distinctive character - an overall sense of enclosure, scale and texture – that is worth protecting.



1889. map of downtown Dobbs Ferry. The red line represents the boundary of the MDR-2 district.



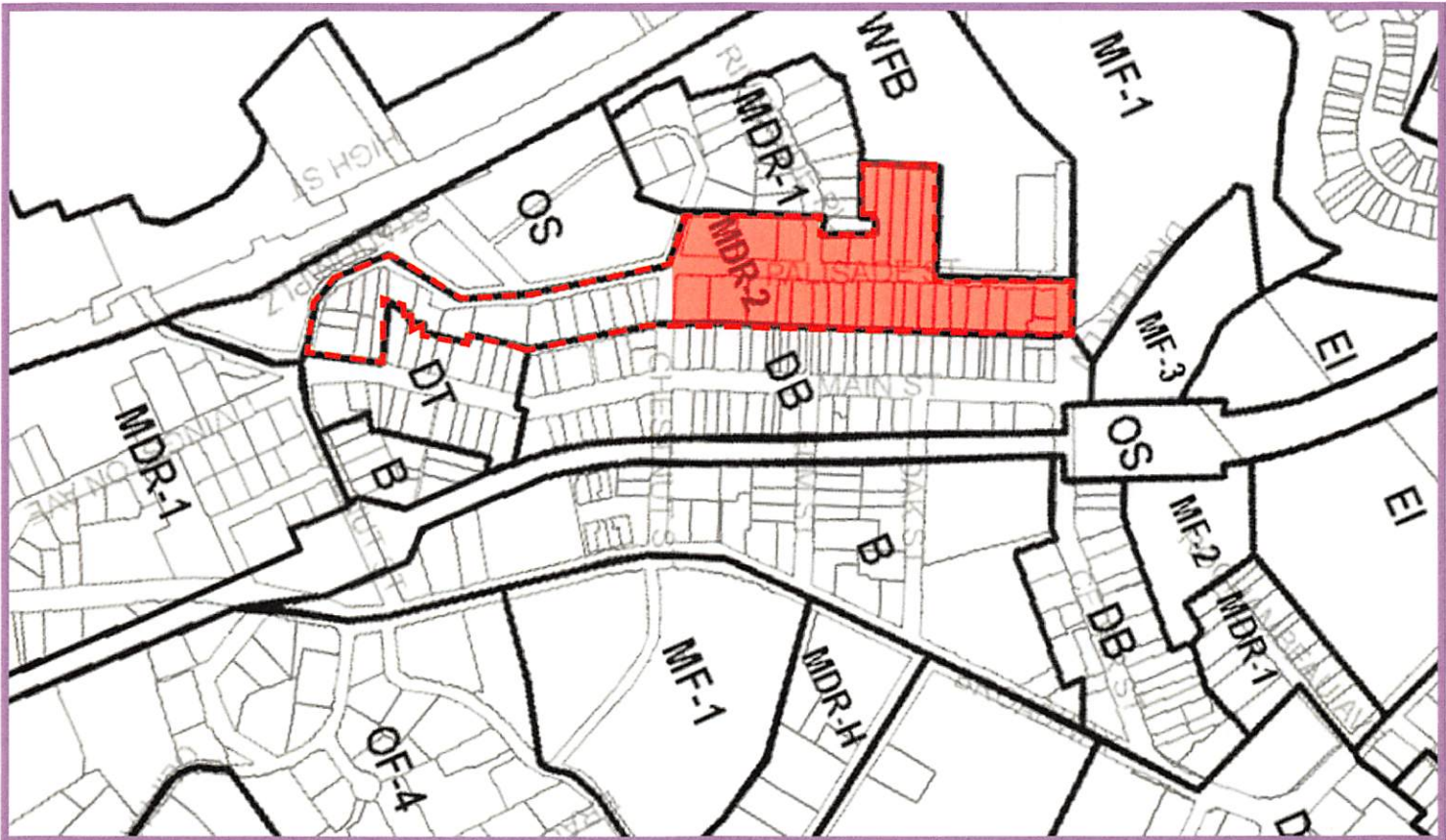
Varied conditions along Palisade Street

These guidelines are meant to capture the character of Palisade Street and to explain it in ways that can inform the development approvals process, both for the applicant and the reviewing boards. These guidelines also support the revised quantitative requirements in Section 300 and accompanying Appendices of the Code.

The guidelines are organized around these five major aspects of Palisade Street character:

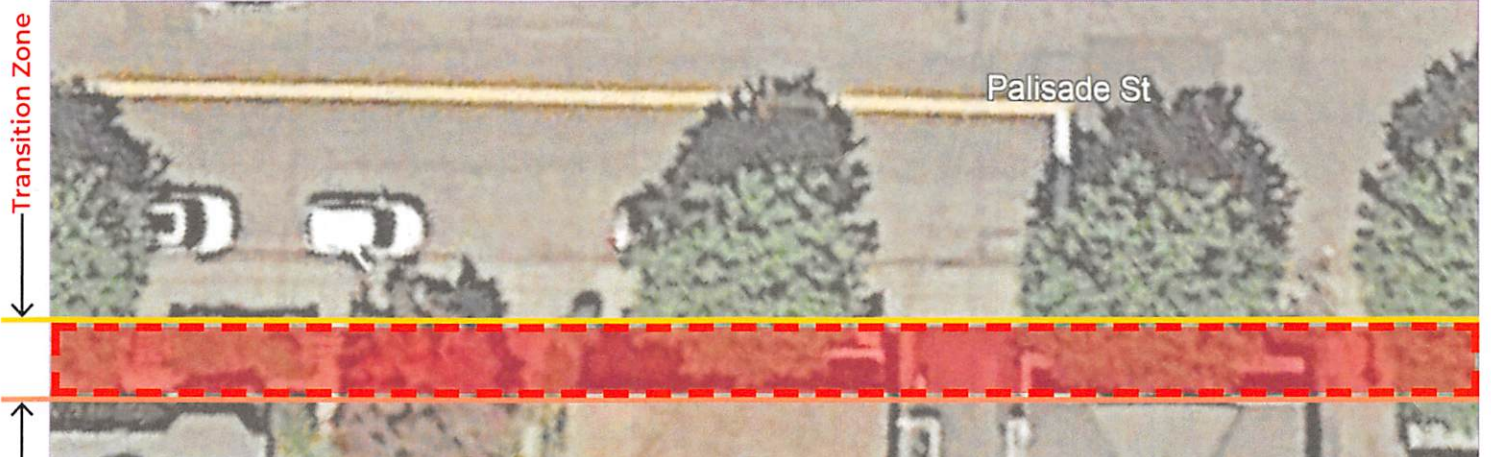
1. There is a transition zone between the public space of the sidewalk and the private space of the entrances.
2. Most of the buildings are small-scale two-story buildings with narrow frontages
3. Elements in the building frontages align horizontally
4. “Punched window” openings predominate
5. Most of the buildings have a similar scale

Part of the motivation for this effort is the need to reexamine the dimensional standards in the MDR-2 Zone (Mixed Density Residential 2). This zone takes in all of Palisade Street with the exception of the two large properties at the north end of the street which are part of the WF-B Zone (Waterfront District B). Several of the provisions in the MDR-2 Zone, especially those regarding coverage, height and the way height is measured, are reexamined. In parallel with the development of these guidelines, revised standards are suggested. Applicants are referred to Section 300 and Appendix B of the Code for these revised dimensional standards.



Zoning map of downtown Dobbs Ferry. The red line represents the boundary of the MDR-2 district. The shaded area is the full study area including the Palisade street edge of the WFB district.

Transition Zone: Elements and Conditions



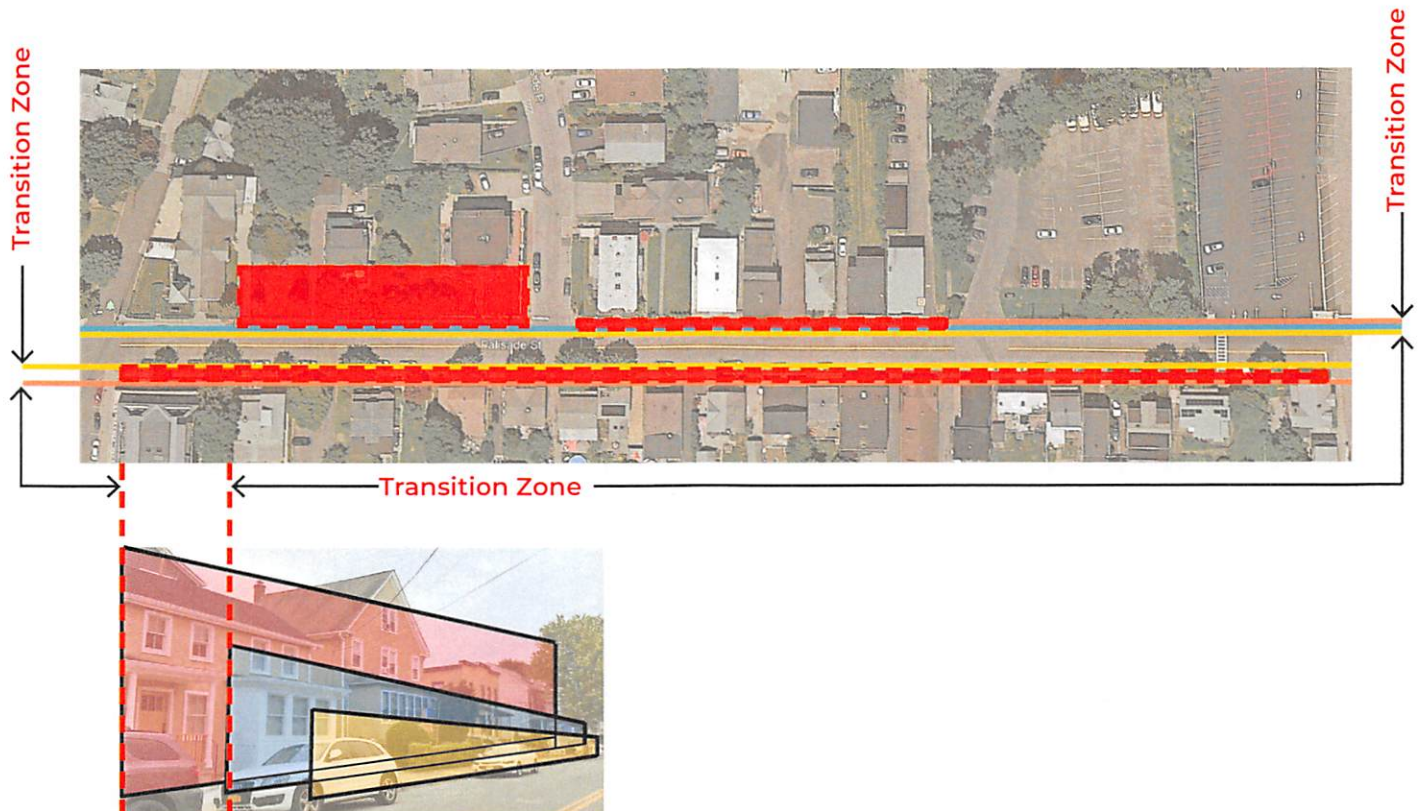
Public to Private Realm

The transition from the public realm of the sidewalk to the private realm of the building entrance is characteristic of Palisade Street. Typically, the principle mass of the building is set back from the sidewalk anywhere from 10 to 15 feet. This space is occupied by smaller-scale elements that mediate between the larger scale of the primary building mass and the pedestrian-scale experience of the sidewalk. The elements in this zone are of different kinds. They include among others:

- open porches
- enclosed porches
- stoops
- planting areas

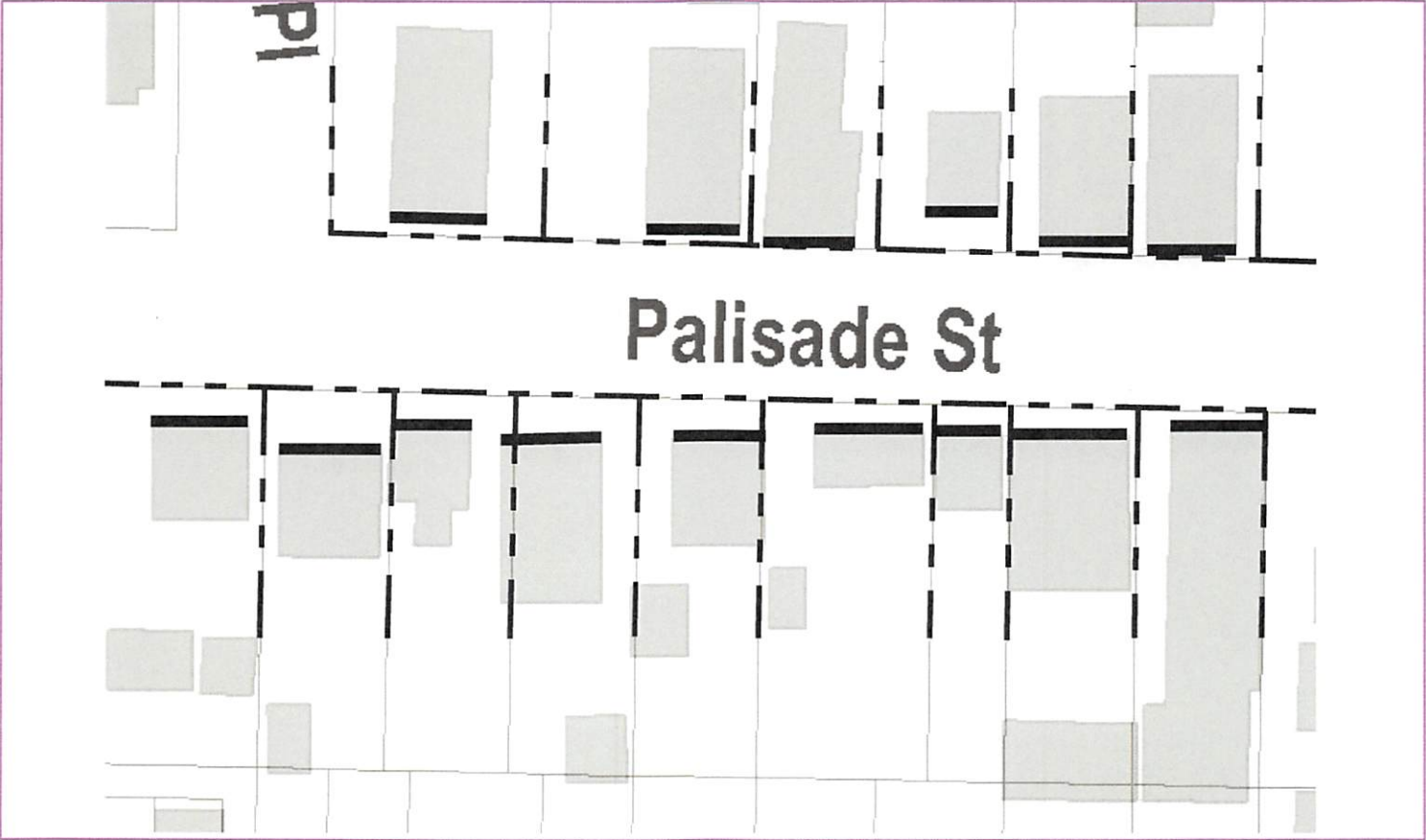
The porches and enclosed porches are all 1-story in height and so they create a horizontal expression line at what is approximately the second floor level. This is one of the two horizontal expression lines characteristic of Palisade Street which are discussed and illustrated in Part 3 of these guidelines (pages 11 and 12).

This condition is not universal: There are some buildings where the principal mass of the building and its entry come right up to the front property line at the sidewalk. But these are the exceptions and the abrupt juxtaposition of the entry and sidewalk is an inferior condition.



Guideline

A 10' to 15' transition zone shall be created between the sidewalk and the primary building mass. The transition zone is not just a front setback – it shall contain smaller scale elements such as single-story porches or planting beds with low walls.



Along Palisade Street, most lots are narrow with small buildings occupying most of the frontage, creating the compact scale of the village downtown.

PART 2 Frontage Scale

Frontage Scale: Street Presence

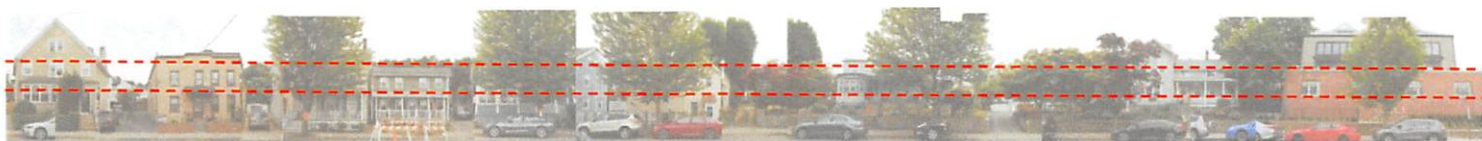
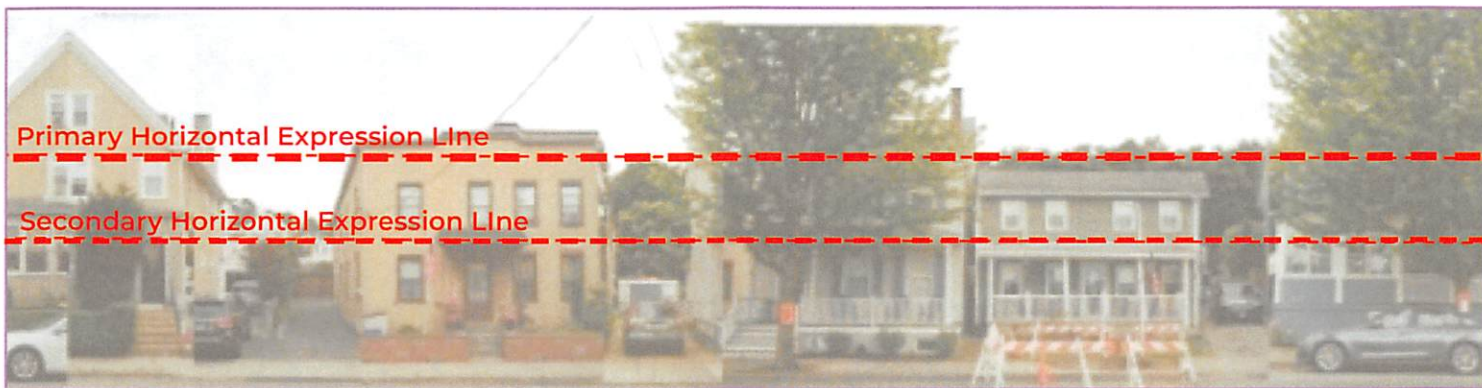
The scale of the frontages that the buildings present to the street are quite uniform along Palisade Street even though there is quite a bit of variation in the frontage width of the lots. There are a few outliers: some lots at the far south end of the study area are quite wide – approximately 80 to 100 feet – as are the lots at the far north end – approximately 60 to 75 feet respectively.

The most typical conditions are found in the central part of the block, in the area outlined in green. Because the lots are narrow, the buildings occupy about 60% to 80% of the lot width. Of the 18 buildings in this most typical area, 12 buildings have frontages of between 25' to 35' and another 6 buildings have frontages of between 35' to 40'. The average frontage is approximately 33'. Because there is a sliding scale that will moderate the size of buildings on the larger lots, 40' is recommended as the maximum frontage for the primary mass of the building.

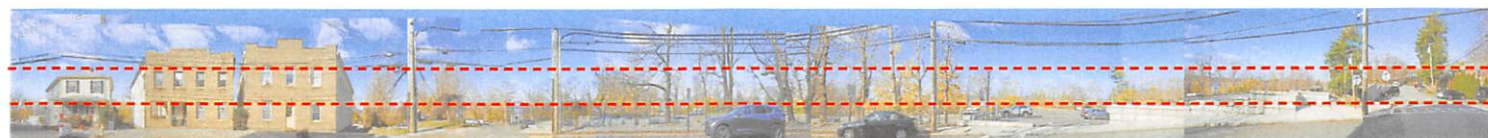
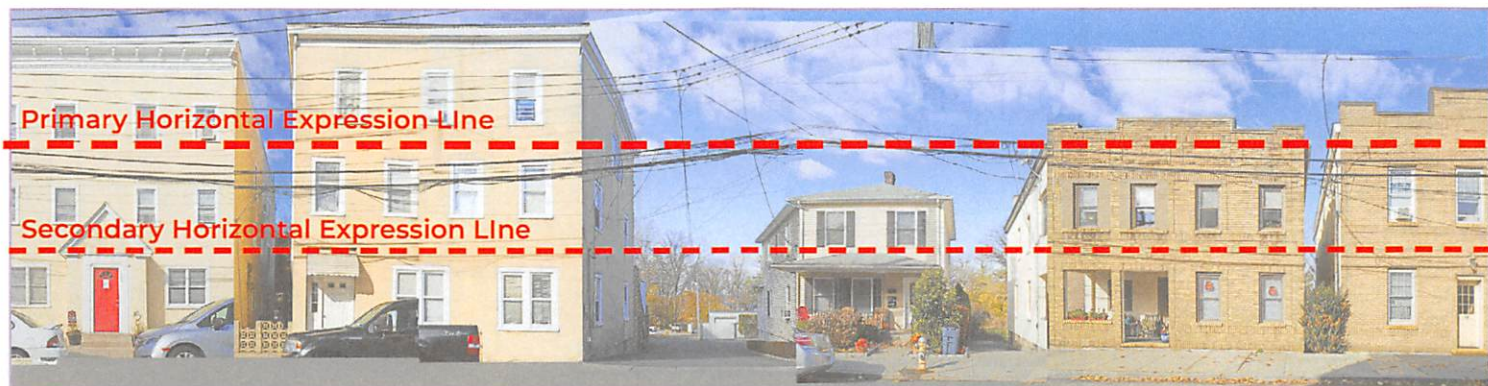


Guideline

The Primary Volume of the building shall be a maximum of 40' in width. For wider sites, where the total building is greater than 40' in width, the additional building frontage shall be set back a minimum of 1 foot with larger setbacks required for larger buildings. (see massing guidelines discussed in Part 5 of these guidelines (pages 15 and 16).



Panorama of east side of Palisade Street. (horizontal expression lines shown in dashed red lines).



Panorama of west side of Palisade Street. (horizontal expression lines shown in dashed red lines).

PART 3 Facade Composition

Horizontal Expression Lines

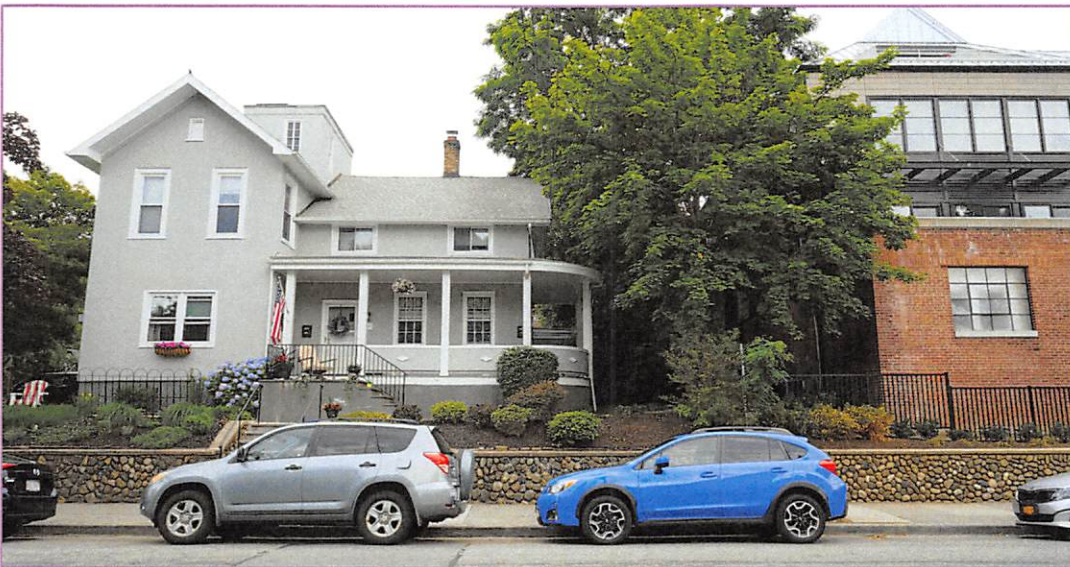
As varied as they are, the buildings along Palisade Street share architectural features that align horizontally, tying the streetscape together. One subtle horizontal expression line, at approximately the heights of the 2nd floors, is created by the tops of the open porches and enclosed porches that are in the Transition Zone (see Part 1 of these guidelines on pages 7 and 8) as well as by bay windows and some other smaller façade elements such as lintels over the first floor windows. A more prominent horizontal expression line corresponds to the top parapets of the buildings with flat roofs or the eave line of the buildings with pitched roofs. New buildings should respond to these expression lines at the 2nd floor and 3rd floor elevations. (see also the massing guidelines in Part 5 pages 15 and 16).



Guideline

Buildings shall create a secondary horizontal expression line at the 2nd floor elevation (approximately 25' as measured from the middle of the Palisade Street frontage). Buildings shall also create a primary horizontal expression line at the 3rd floor elevation which can be either the eave of the pitched roof or a setback between the 2nd and 3rd floors of a flat-roofed building (approximately 37' as measured from the middle of the Palisade Street frontage).

Typical Facade: Punched Windows



Typical punched window facades

PART 4 Facade Composition

Punched Windows

Most of the facades along Palisade Street have so-called “punched windows”: the facades have more solid surface area than open, giving the impression that the windows have been “punched” out of a solid wall. The windows are taller than wide, often double hung with divided lites. New facades should have compatible proportions and rhythm. (See also the Downtown Historic District guidelines for other parameters and guidelines related to fenestration).

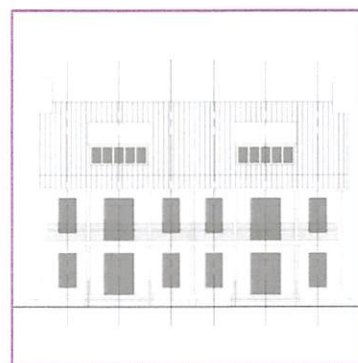
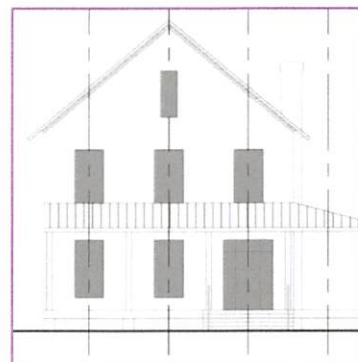
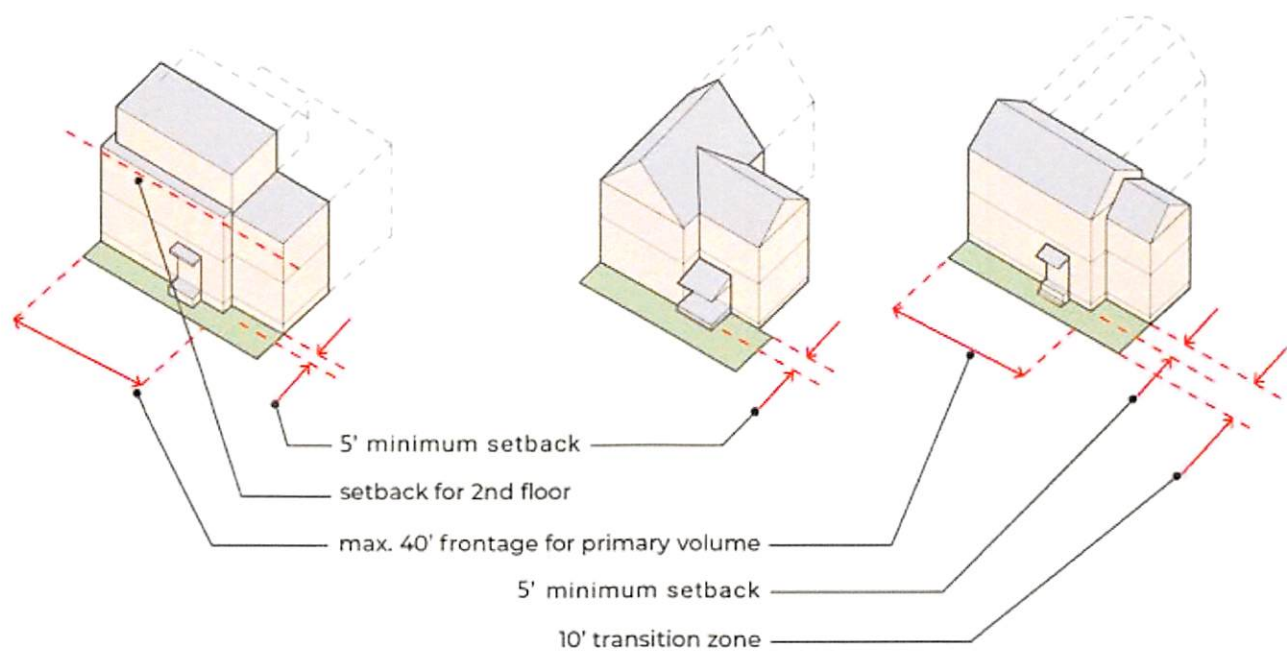


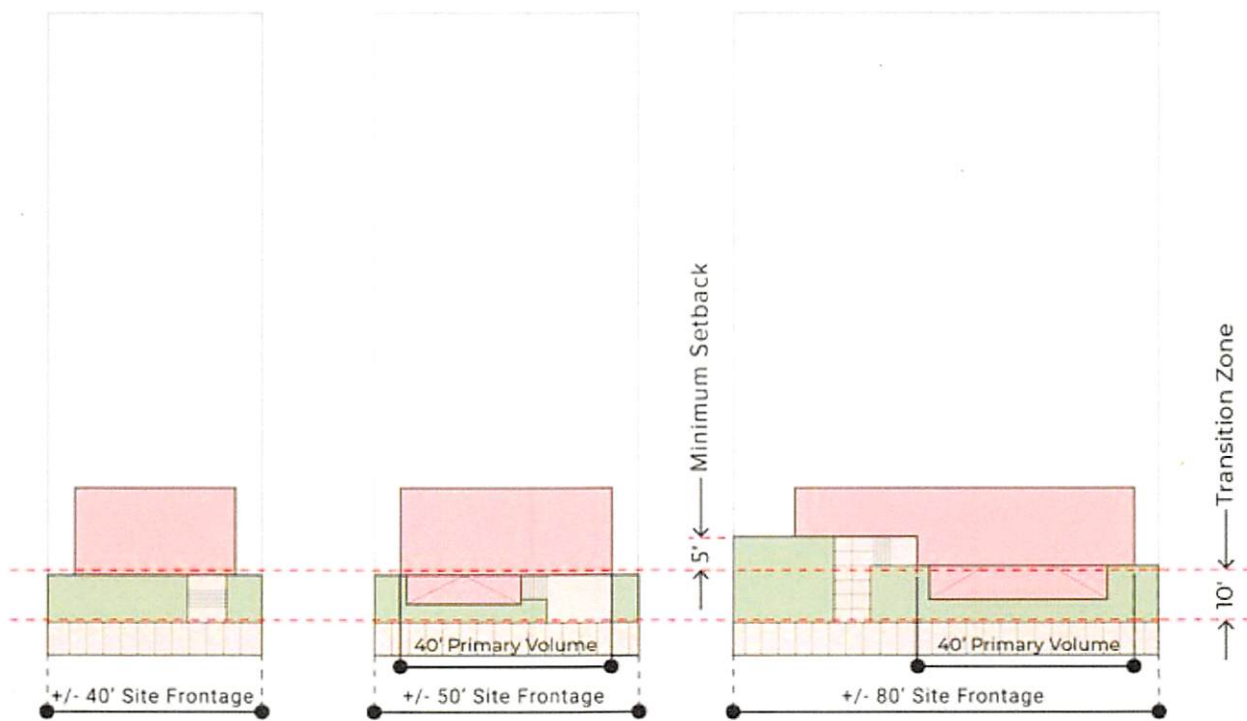
image courtesy of Hart Howerton Architects

Guideline

Building window openings shall be “punched openings” with a scale and pattern similar to the other buildings on Palisade Street. The total area of window openings shall not exceed 50% of the total wall area of the primary volume of the building.



Massing guidelines- typical frontage conditions



Massing guidelines- typical frontage conditions

Massing and Scale

While there are some outliers, the majority of the buildings along Palisade Street are similar in terms of overall massing and scale.

In terms of coverage, an analysis was done of the coverages of the existing buildings in the most typical, core study area. There is variation even within the study area. Rather than simply finding the average, a coverage guideline of 40% insures that three quarters of the existing properties conform with the new zoning standard. As elsewhere, the contextual zoning provides a sliding scale that calibrates the base coverage for sites of different sizes. (See Section 300 and Appendix B of the Code).

In terms of massing, the scale of the street is created by buildings that consist typically of a single, primary volume which is generally two stories high. Typically, there is a pitched roof facing the street or parallel to the street although there are several flat-roofed buildings as well. As is typical for buildings of this vintage, there are often additions and extensions that have been added over time. But these additions are always clearly secondary to the principal volume of the building.

To maintain the sense of scale of the street, it should always be possible to read a primary volume that is no wider than 40'. Additions and extensions shall be clearly set back from the 40' frontage of the primary volume. This should be a minimum of 5 feet for buildings where the secondary massing has a frontage of between 10 and 12 feet and become progressively deeper for buildings where the frontage of the secondary volume is wider than 10 or 12 feet.

This still enables larger buildings because, within the coverage constraints, it is possible to add to the sides of the primary volume; or, because these properties are so much deeper than they are wide, to add onto the back of the primary volume.

Buildings shall be a maximum of three stories in height. To create the characteristic horizontal expression lines discussed above, flat roofed buildings shall have a setback between the second and third floors. Pitched roof buildings, where the third floor living space is created by dormers, shall have an eave between the second and third floors.

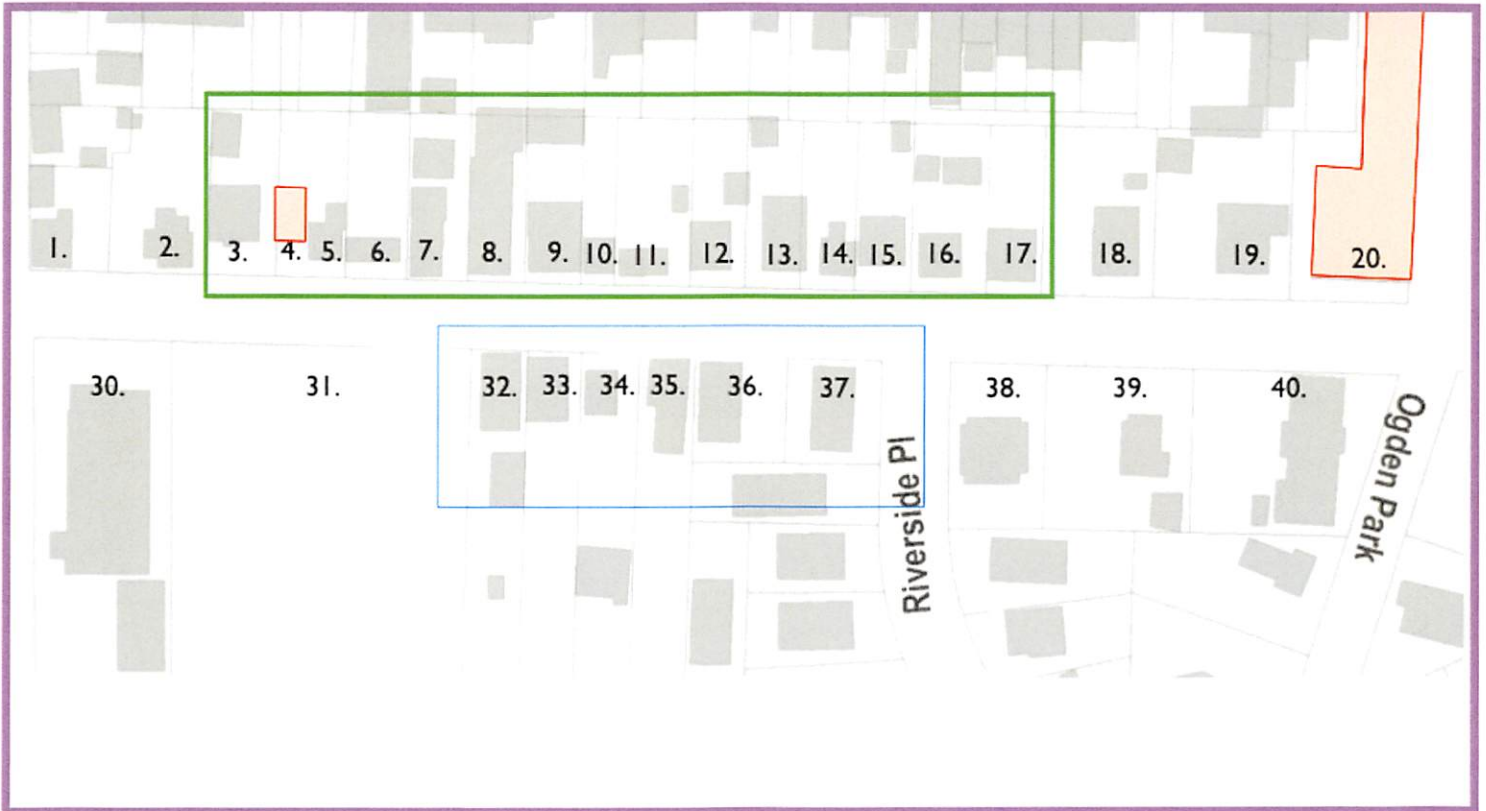
(see Section 300 and Appendix B of the Zoning Code for dimensional standards)

Guideline

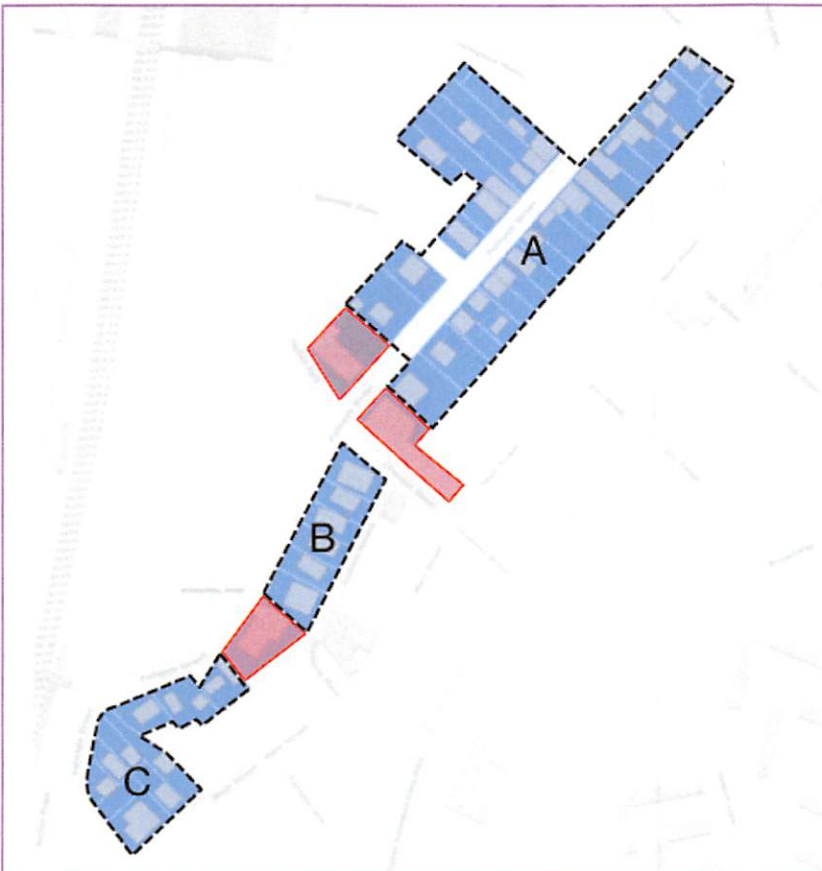
- There shall be a clearly articulated Primary Volume. Other massing shall be secondary in scale and set back from the Primary Volume.
- Maximum Frontage for the Primary Volume shall be 40'.
- A setback shall be created between the 2nd and 3rd floors.
- Smaller massing elements at the first floor level shall help articulate the transition from the sidewalk to the Primary Volume.

Appendix 1: Frontages Map

*See Appendix: Key Chart with Frontages, opposite



Appendix 2: Coverages Map



MDR2 Coverages

Overall

MDR 2 .29

By Subarea

Subarea A .28

Subarea B .35

Subarea C .28

Appendix 1: Frontages Chart

*See Appendix: Key Map with Frontages, opposite

Frontages- East

Key Number	lot width	frontage	ratio	
1	58'	26'	45%	
2	75'	39'	52%	
3	45'	34'	75%	
4		21'		
	50'	46'	90%	
5		25'		
6	48'	40'	85%	
7	42'	26'	62%	
8	42'	29'	70%	
9	45'	39'	87%	
10	23'	25'	87%	
11	58'	37'	64%	
12	40'	32'	80%	
13	38'	33'	87%	
14	42'	27'	64%	
15	42'	30'	71%	
16	58'	34'	59%	
17	42'	39'	93%	
18	80'	35'	45%	
19	105'	40'	38%	
20	85'	76'	89%	
	Ranges:	25' to 30'	7	
		30' to 40'	12	
	Average:	590' /18	33'	

Frontages- West

Key Number	lot width	frontage	ratio		
30	100'	NA	NA		
31	220'	NA	NA		
32	42'	30'	72%		
33	42'	32'	76%		
34	40'	22'	55%		
35	40'	28'	70%		
36	65'	34'	52%		
37	65'	34'	52%		
38	74'	40'	54%		
39	100'	35'	35%		
40	145'	48'	33%		
	Ranges:	20' to 30'	2		
		30' to 50'	7		
	Average:	303' /9	34'		

June 2021



PALISADE STREET: CHARACTER GUIDELINES

This document was created for the Village of Dobbs Ferry by Robert Lane and Steve Brosnahan of the Planning Board and Lair Rosenkranz of the Architectural and Historic Review Board with the advice and support of other members of the Planning Board, Village Board Liaison Donna Cassell and Village Planner Dwight Douglas.

Graphic design templates provided by Stephen Tilly, Architect.