



## **VILLAGE OF DOBBS FERRY BOARD OF TRUSTEES AGENDA**

**MEETING DATE:** OCTOBER 26, 2021

**AGENDA ITEM SECTION:** DISCUSSION ITEMS

**AGENDA ITEM NO. :** 2

**AGENDA ITEM:** APPLICATION FOR MULTI-MODAL GRANT OPPORTUNITY REVISED DESIGN FOR STORM STREET CROSSWALK

**ITEM BACKUP DOCUMENTATION:**

1. MEMORANDUM DATED OCTOBER 21, 2021 FROM OFFICE OF THE VILLAGE ADMINISTRATOR TO MAYOR ROSSILLO AND THE BOARD OF TRUSTEES
2. ASHFORD AVENUE CONCEPTUAL PLAN
3. ASHFORD AVENUE REALIGNMENT-BUMP OUTS 3 CORNERS

# Village of Dobbs Ferry

**Mayor**  
Vincent Rossillo

**Board of Trustees**  
Donna Cassell – Deputy Mayor  
Maura Daroczy  
Christy Knell  
Michael Patino  
Nicole Sullivan  
Lawrence Taylor



## MEMORANDUM

**Village Administrator**  
Richard Leins

**Village Treasurer**  
Jeff Chuhta

**Village Clerk**  
Elizabeth Dreaper

**Village Justice**  
David Koenigsberg

**To:** Mayor Rossillo and the Board of Trustees

**From:** Office of the Village Administrator

**Re:** Revised concept plan for Multi-Modal grant

**Date:** October 21, 2021

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In February 2020, Assemblymember Tom Abinanti's office offered the Village an opportunity to apply for an \$80,000 Multi Modal grant through the New York State Department of Transportation to promote pedestrian safety. The program provides reimbursed funding for transportation capital projects including those that enhance pedestrian safety. The program does not require matching funds from the Village, but is reimbursement-based and therefore we would need to finance the project implementation and then seek reimbursement for the project that has been pre-approved by the state.

In May 2021, the Board was presented with a design to improve the Ashford Avenue crossing at Storm Street. Ashford Avenue at the intersection with Storm Street widens significantly and is situated on a curve at the crest of a hill that can obscure driver sightlines. The crossing is regularly used by children walking to school. The Traffic Committee identified improvements to this intersection as the top infrastructure priority this year and the Our Streets pedestrian safety group identified this intersection as a point of concern in their September Community Walk audit.

The Board approved a resolution to move forward with the application in May 2021. The concept plan for the design has been refined in the intervening months. Instead of a pedestrian refuge, the concept plan now calls for curb bump outs and a flashing beacon. There are now two plans to consider. Notes from Traffic Engineer Russell Scott of Nelson Pope Engineering are included below, and Mr. Scott will be available to answer questions.

We look forward to your feedback on this concept for the Storm Street crossing.

**Excerpted Notes from Russell Scott, Traffic Engineer, Nelson Pope Engineering:**

Observations:

- Most of the pedestrian traffic in the AM is from the north side of Ashford crossing Storm Street and but there is still significant usage of the crosswalk on Ashford Avenue
- There is very little vehicular traffic entering and exiting Grandview Avenue
- There is a heavy right turn movement from Ashford Avenue to Storm Street and the vehicles use the extra roadway width to drive around cars that are backed up waiting for the traffic signal at Walgrove Avenue. This creates a dangerous condition for the pedestrians and crossing guard crossing Storm Street
- The sight distance exiting Storm Street is limited and requires the vehicles to pull up past the stop bar and crosswalk to see vehicles traveling along Ashford Avenue. This creates an unsafe condition for the pedestrians as they sometimes have to cross behind the vehicle.
- The site distance is limited headed west over along Ashford Avenue as we have discussed previously.
- Vehicles use the entire roadway width of Ashford Avenue west of Storm/Grandview for queuing of the through lane and right turn lane at Walgrove

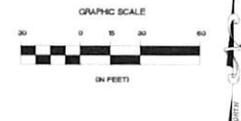
Solutions in the concept plan:

- Improves the sight distance for cars exiting Storm Street and moves the stop bar and crosswalk closer to Ashford
- Reduces the speed and stacking of side by side cars making a right turn onto Storm and improves the visibility of the crosswalk
- Keeping the cross walk west of the intersection, allows drivers more reaction time and distance to see pedestrians in the crosswalk, the crest of the hill east of the intersection makes it difficult to see.
- Reduces the crossing distance for pedestrians crossing Ashford Ave

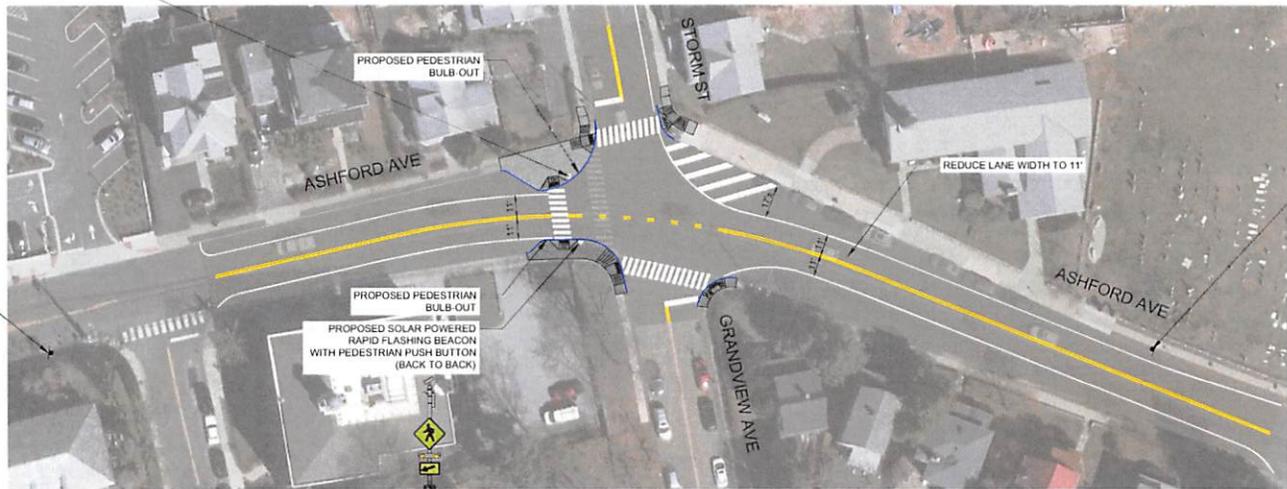
There is a balance of creating a queue/back of traffic and pedestrian safety. I do have a slight concern traffic queuing concern through during the peak periods...this may warrant further discussion.



EXISTING CONDITIONS



- GENERAL NOTES:**
1. ALL SIGNS AT CROWNWALK SHALL BE POINTED BACK TO BACK.
  2. ALL SIGNS SHALL BE MOUNTED TO THE FACE OF THE SIGN WITH A 4" X 4" VISIBLE BACKING AND A 1/2" CLEARANCE FROM THE MOUNTING SURFACE TO THE SIGN FACE.
  3. THE SIGN (ADVANCED SIGN) SHALL BE PLACED AT APPROXIMATELY 300'.
  4. THE PROPOSED SIGN SHALL BE MOUNTED ON A SINGLE CHECKERBOARD POST AS NECESSARY WITH A HIGH VISIBILITY STRIPES ON THE POST.



CONCEPTUAL PLAN

- PROPOSED SIGN (SEE NOTE 2)
- 
- W11-2  
(36"x36")  
W16-9P  
(24"x12")

LEGEND	
PROPOSED SIGN	
NEW/UPGRADED PAVEMENT MARKINGS	
RECTANGULAR RAPID FLASHING BEACON (RRFB)	
CONCRETE SIDEWALK	
PROPOSED CURB	

PROPOSED SOLAR POWERED RAPID FLASHING BEACON WITH PEDESTRIAN PUSH BUTTON (BACK TO BACK)



PROPOSED SIGN (SEE NOTES 2)

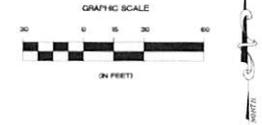


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NO.	DATE	REVISION	BY			

**NELSON+POPE**  
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70 Maxx Road, Melville, NY 11747 • 631.427.5665 • nelsonpopeny.com



EXISTING CONDITIONS



GENERAL NOTES:

1. ALL SIGNS AT CROSSWALK SHALL BE POSTED BACK TO BACK.
2. ALL SIGNS SHALL BE MOUNTED TO CURB (WITH A HIGH VISIBILITY BACKGROUND) & LEGIBLE PER THE MUTCD AND NYSP DMV'S RECOMMENDATIONS.
3. PROPOSED SIGNAGE SIGNS SHALL BE PLACED AT APPROXIMATELY 500 FT.
4. THE PROPOSED SIGN SHALL BE INSTALLED ON A SINGLE OR DOUBLE POST AN NECESSARY WITH A HIGH VISIBILITY TUBES ON THE POSTS.

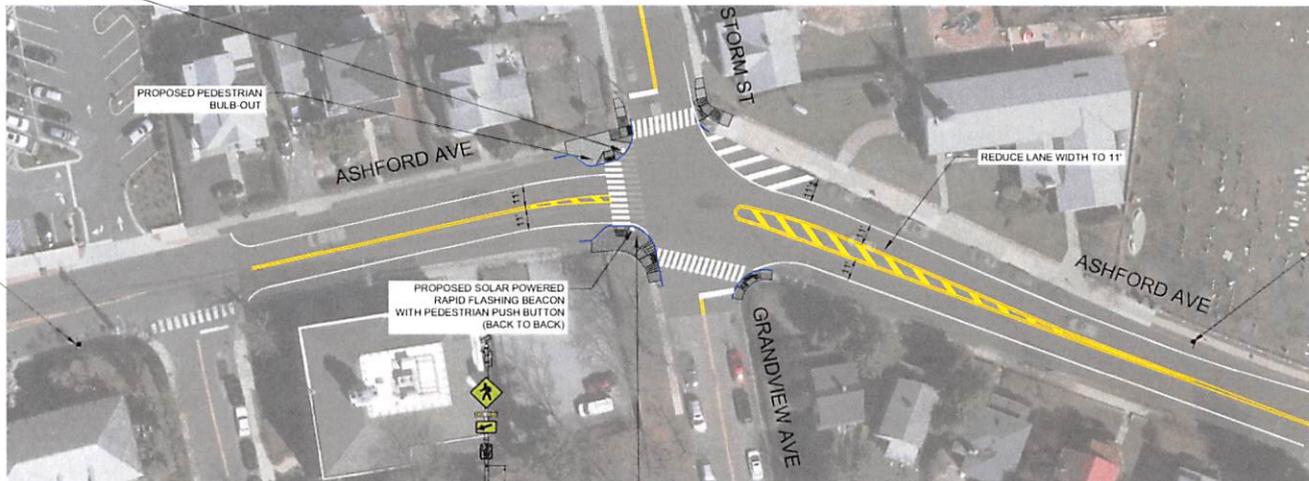
PROPOSED SOLAR POWERED  
RAPID FLASHING BEACON  
WITH PEDESTRIAN PUSH BUTTON  
(BACK TO BACK)



PROPOSED SIGN  
(SEE NOTE 2)



W11-2  
(36"x36")  
W16-5P  
(24"x12")



CONCEPTUAL PLAN

PROPOSED SIGN  
(SEE NOTE 2)



W11-2  
(36"x36")  
W16-5P  
(24"x12")

LEGEND

PROPOSED SIGN



NEW/UPGRADED  
PAVEMENT MARKINGS



RECTANGULAR RAPID  
FLASHING BEACON (RRFB)



CONCRETE SIDEWALK



PROPOSED CURB



NO.	DATE	BY	REVISION

CLIENT: VILLAGE OF DOBBS FERRY  
112 MAIN STREET  
DOBBS FERRY, NY 10522

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DESIGNED BY	11	DRAWING TITLE	CONCEPTUAL PLAN ALTERNATIVE 2 ASHFORD AVE / GRANDVIEW AVE / STORM ST VILLAGE OF DOBBS FERRY, NEW YORK
CHECKED BY	16	DRAWING NUMBER	CP-2
DATE	August 26, 2023	SHEET #	2 OF 2
SCALE	AS SHOWN		



EXISTING CONDITIONS



GENERAL NOTES:

1. ALL SIGNS AT CURBSIDE SHALL BE POSTED BACK TO BACK
2. ALL SIGNS SHALL BE MANUFACTURED WITH A HIGH-VISIBILITY BACKGROUND & LEGIBLE PER THE MUTED AND NON-SLIP PLATE WITH RECOMMENDATIONS
3. PROPOSED ADVANCE SIGNAGE SHALL BE PLACED AT APPROPRIATELY 300'
4. THE PROPOSED SIGN SHALL BE INSTALLED ON A SINGLE OR DOUBLE POST AS NECESSARY WITH A HIGH-PRIORITY SIGN ON THE POSTS.

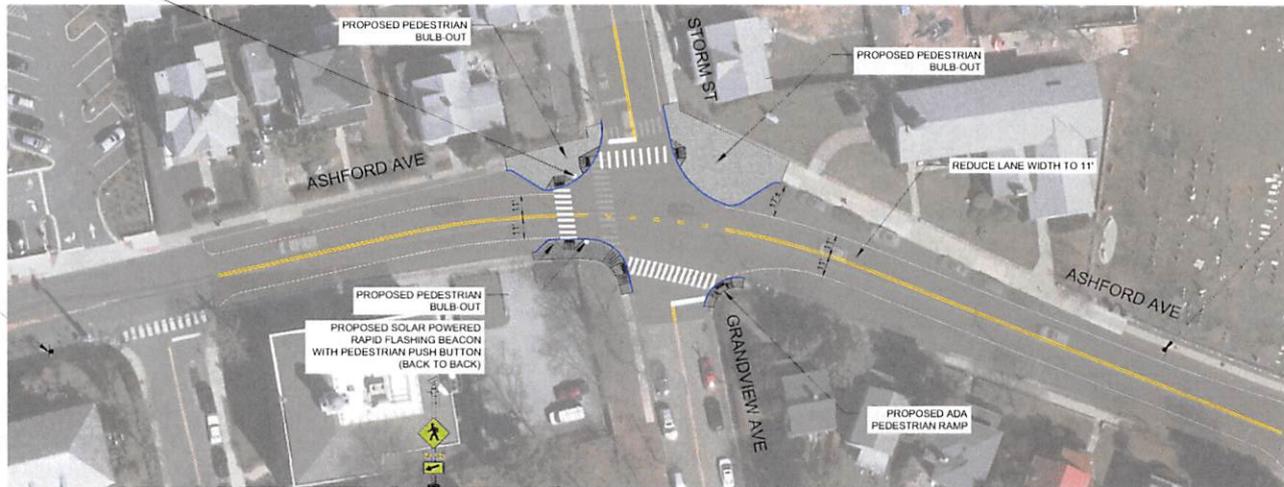
PROPOSED SOLAR POWERED RAPID FLASHING BEACON WITH PEDESTRIAN PUSH BUTTON (BACK TO BACK)



PROPOSED SIGN (SEE NOTES 2)



W11-2 (36"x36")  
W16-9P (24"x12")



CONCEPTUAL PLAN

PROPOSED SIGN (SEE NOTE 2)



W11-2 (36"x36")  
W16-9P (24"x12")

LEGEND

- PROPOSED SIGN
- NEW/UPGRADED PAVEMENT MARKINGS
- RECTANGULAR RAPID FLASHING BEACON (RRFB)
- CONCRETE SIDEWALK
- PROPOSED CURB

No.	DATE	REVISION	BY

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CONTRIBUTOR

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CLIENT

VILLAGE OF DOBBS FERRY  
112 MAIN STREET  
DOBBS FERRY, NEW YORK 10522

DRAWN BY: E1  
CHECKED BY: H5  
DATE: October 19, 2021  
JOB NO.: 2021-001

DRAWING TITLE

**CONCEPTUAL PLAN  
ALTERNATIVE 1**  
ASHFORD AVE / GRANDVIEW AVE / STORM ST  
VILLAGE OF DOBBS FERRY, NEW YORK

DRAWING NUMBER

**CP-1**

SHEET 1 OF 1