

PROPOSED
MAIN STREET SIDEWALKS IMPROVEMENT PROJECT

VILLAGE OF DOBBS FERRY, WESTCHESTER COUNTY, NY
(FEDERAL AID PROJECT)


















GENERAL NOTES

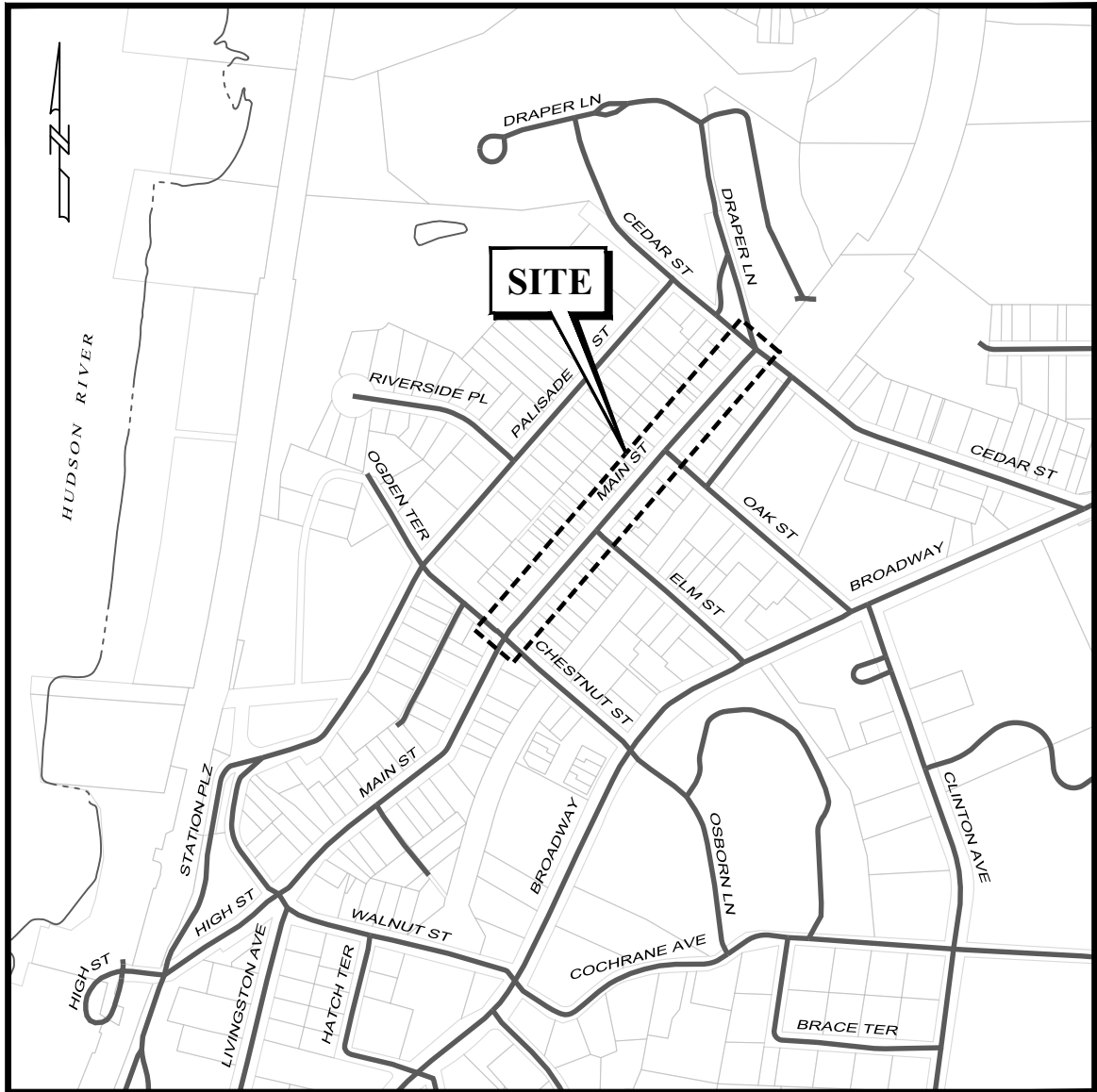
- All materials and methods of construction shall be in accordance with these plans, specifications, any revisions thereto, New York State Department of Transportation (NYSDOT) Standard Specifications (USC, January 1, 2022 or most current official version), and the rules and regulations of the Village of Dobbs Ferry.
- The subsurface information shown hereon is not guaranteed as to accuracy or completeness. The Contractor shall verify the location of all existing utility lines whether in public right-of-way, in easements, or in private property prior to starting any excavation and shall be responsible for the same. The Contractor shall be responsible for contacting the Underground Facilities Protective Organization, pursuant to N.Y.S. Law, 16 NYCRR Code 753.
- The Contractor shall be responsible for the relocation of any utility service line or valve which is in interference with the proposed work. Public utility work is reimbursable, private utility work shall be done by the utility owner and is not reimbursable.
- The Contractor shall verify all existing and proposed elevations in the field.
- The Contractor shall provide the Engineer with a telephone number of the person responsible in the case of an emergency, 24 hours a day, 7 days a week.
- All damage to public or private facilities caused by the Contractor's operation shall be repaired to the satisfaction of the Owner at the Contractor's expense.
- The Contractor shall submit shop drawings for all highway works to the Engineer. No construction shall be allowed until the shop drawings are approved.
- The Contractor shall comply with O.S.H.A. Standard 29 CFR - Part 1926.650, .651 and .652 for all excavations.
- Refuse from demolition shall become the property of the Contractor. It shall be the Contractor's responsibility to dispose of all construction refuse legally.
- The Contractor shall prevent the formation of any low spots where water can collect behind new curb or sidewalk and any possible redirection of runoff onto private property and shall take whatever corrective measures are necessary. The Contractor is responsible, at no cost to the Owner, to correct any deleterious water ponding areas.
- Existing manhole covers to remain shall be adjusted by the Contractor to finished grades where required. Castings shall be recessed 1/4 inch below the finished pavement elevation.
- Existing water and gas valves to remain shall be adjusted by the utility company to finished grades where required. Castings shall be recessed 1/4 inch below the finished pavement elevation.
- Existing pavement shall be sawcut in a straight line where it is to be bounded by new pavement.
- The pavement and subbase thicknesses noted on these plans are after compaction.
- All existing site features (including but not limited to: building facades, pavement, driveway aprons, curbing, grass, landscaping, piping, utility lines, signs, walls, fencing and structures) disturbed or damaged by construction shall be restored by the Contractor to a condition equal to or better than those currently existing and as directed by the Engineer.
- Disturbed grass or earth areas shall be provided with 4 inches minimum of topsoil and seeded or sodded as described in the specifications.
- Any damaged pedestrian indicators or signal infrastructure shall be repaired and/or replaced to current standards, including ADA/PROWAG requirements at no separate cost.
- Prior to the beginning of any preliminary construction work, the Contractor, Village representative, NYSDOT, and the Engineer shall meet on-site to walk the roadways proposed for improvements. During the "walk-through", the Contractor will be required to markup all roadways, pavements, etc. to be sawcut.
- The Village's engineering consultant may require the installation of erosion control measures if deemed appropriate to mitigate unforeseen siltation or erosion of disturbed soils. Erosion control measures shall be installed in accordance with the latest edition of N.Y.S.D.E.C publication "New York State Standards and Specifications for Erosion and Sediment Control". (Items 209.13 & 209.1703)

N.Y.S.D.O.T. NOTES

- The latest revisions of the standard sheets maintained by the department, which are current on the date of advertisement for bids, shall be considered to be in effect. All pay items and work contained in the contract and any additional pay items and work encountered during the course of the contract shall be subject to the applicable standard sheet(s) unless otherwise specified in the contract documents.
- All work contemplated under this contract is to be covered by and in conformity with the standard specifications (US Customary) referenced in the contract project "proposal" except as modified by these plans or by changes set forth in the contract project "proposal."
- Contract plans have been designed in accordance with NYSDOT policies and guidelines and the Final Design Report approved on 07/31/2019.

LEGEND

---	HIGHWAY BOUNDARY LINE		EXIST. FIRE HYDRANT
---	PROPERTY LINE (ADJOINING)		EXIST. SPRINKLER CONNECTION
----- 140 -----	EXIST. CONTOUR LINE		EXIST. GAS LINE
----- 149.7 -----	EXIST. SPOT ELEVATION AT DOOR SILL		EXIST. GAS VALVE
----- ST -----	EXIST. STORM PIPE		EXIST. UTILITY POLE
	EXIST. CATCH BASIN		EXIST. UTILITY POLE W/ LIGHT
	EXIST. DRAINAGE MANHOLE		EXIST. TELEPHONE MANHOLE
	EXIST. DOWNSPOUT & ROOF LEADER		EXIST. MAILBOX
----- SS -----	EXIST. SANITARY SEWER LINE		EXIST. BOLLARD
----- S -----	EXIST. SANITARY SEWER MANHOLE		EXIST. PARKING METER
----- W -----	EXIST. WATER LINE		EXIST. SIGN
	EXIST. WATER VALVE		PROP. STREET TREE
			EXIST. DECIDUOUS TREE



LOCATION MAP

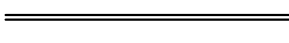
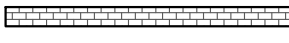

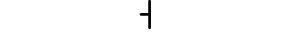

SCALE: 1" = 500'

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES (Item 619.01)

- The maintenance and protection of traffic schemes, both pedestrian and vehicular, shall be the responsibility of the Contractor. Access to all commercial, institutional and residential parking facilities shall be maintained at all times. A safe means of pedestrian access to and from all points within the contract limit shall be provided.
- Where excavations or other work occur on or near sidewalks or other pedestrian ways, the Contractor shall provide a safe and orderly pedestrian passage that complies with ADA standards around or through the work area. The Contractor shall submit a pedestrian-bicyclist detour plan to the Engineer for approval. The pedestrian passage shall not subject pedestrians to hazards from traffic or construction operations nor cause the pedestrians to walk upon unsuitable or hazardous surfaces. Construction materials, vehicles, equipment, debris, temporary sign supports, or other materials shall not be placed or stored on open sidewalks or walkways unless expressly shown in the Contract Documents or approved by the Engineer. Upon completion of the work at each location, the Contractor shall remove all remaining material and equipment and shall leave the affected area(s) in a neat condition.
- The Contractor must submit traffic maintenance and staging schemes in writing to the Engineer for approval.
- All traffic maintenance devices, including, but not limited to, temporary signs, barricades, steel plates, lights and warning signals, shall be constructed and displayed within the Highway Boundary (public right-of-way).
- All diverted traffic, pedestrian and vehicular, shall remain within the Highway Boundary (public right-of-way).

RIGHT-OF-WAY NOTES

- All work to be performed under this contract will be within the public right-of-way (ROW) in accordance with Section 105-15 of the Standard Specifications. The Contractor is to assure himself that all work is being performed within the ROW, including but not limited to vehicle access; storage of equipment, materials, debris and waste; landscaping; vegetation removal and management; grading, seeding and the installation of turf; and the installation of any fences or protective barrier.
- If Contractor is unable to identify the limits of the rights-of-way when the contract calls for work in those vicinities, the Contractor must contact the Project Engineer for definitive boundary determinations before any work may be initiated at those locations (Standard Specifications Sections 105-10 and 625).
- In accordance with Section 107-13 of the Standard Specifications, releases for any non-essential contract work outside of the existing rights-of-way, including plantings, landscaping or driveway enhancement, will be provided by the Project Engineer and in no instance are to be secured by the Contractor. The Contractor shall not invade upon private properties, lands or buildings outside of the rights-of-way for any reason without first securing written permission from the property owner (Standard Specifications Sections 105-15, 107-13).
- The Contractor will be held liable for any damages done. Any such injuries or damages shall be satisfactorily repaired or items replaced at the Contractor's expense (Standard Specifications Section 107-08 and 107-13).

	PROP. GRANITE CURB
	PROP. COLORED & IMPRINTED CONC. EDGING
	PROP. PARKING METER
	PROP. SIGN
	PROP. STREET TREE

SPECIAL NOTE
COORDINATION WITH THE UTILITY SCHEDULE - NO ANTICIPATED INVOLVEMENT
P.I.N. 8761.52

Utility facility adjustments and/or relocations are not anticipated for this project. If the Village determines that utility facility adjustments and/or relocations will be necessary, the provisions of Section 102-09 Other Contracts, Coordination and Access will apply.

Any such adjustments and/or relocations will be performed by the Utility owners and/or the state contractor upon direction by the State Engineer-In-Charge.

Suitable time frames for these additions shall be coordinated between the State, the Contractor, and the affected Utility.

The contractor is governed by and must adhere to the provisions of 16 NYCRR Part 753 (Protection of Underground Facilities).

FIRE DEPARTMENT CONCRETE APRON NOTES

- Concrete shall be NYSDOT Class D, 5,000 psi at 28 days, with microfiber.
- Steel reinforcement shall be epoxy coated Grade 60 No. 5 deformed bars with longitudinal and transverse spacing a maximum of 12 inches on center.
- All materials and methods to be in conformance with the project specifications.
- JOINT CONSTRUCTION: Construct expansion, control, and construction joints straight with face perpendicular to concrete surface. Run slab reinforcement continuous through joint. See concrete joint details.
- CONTROL JOINTS: Provide joints at equal spacing throughout the slab, maximum of 20'-0" on center each way. A joint shall be installed between bays.
- CONSTRUCTION JOINTS: Place construction joints at end of placements and at locations where placement operations are stopped for a period of more than a 1/2 hour, except where such placements terminate at expansion joints. Construct joints in accordance with standard details.
- EXPANSION JOINTS: Locate expansion joints as shown on plans and at concrete curbs, catch basins, manholes, inlets, structures, sidewalks, and other fixed objects. Provide premolded joint filler for expansion joints.
- Concrete shall not be placed in the pour gap until 72 hours after placement of the most recent adjacent section.
- Contractor shall meet existing grades at garage slab to ensure fire truck entry.
- Subgrade to be approved by engineer prior to placement of concrete.
- The Contractor to verify clearance of fire trucks into the garage bays based on the proposed grades of the driveway apron.

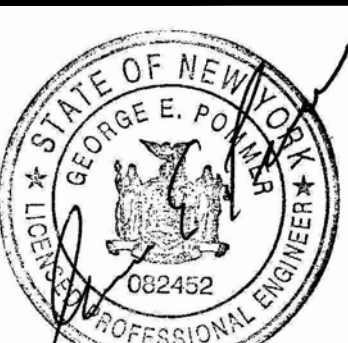

TABLE OF MAINTENANCE JURISDICTION					
HIGHWAY	LIMITS (STATION)	CENTERLINE (MILES)	LANE (MILES)	FEATURES TO BE MAINTAINED	JURISDICTION
MAIN STREET	0+00 TO 10+60	0.201	0.402	ALL FEATURES, INCLUDING ROADWAY PAVEMENT, SIDEWALKS, DRAINAGE, STREET TREES, AND ROADWAY & SIDEWALK SNOW REMOVAL	VILLAGE OF DOBBS FERRY SECTION 40, SUBDIVISION 18 OF N.Y. HIGHWAY LAW

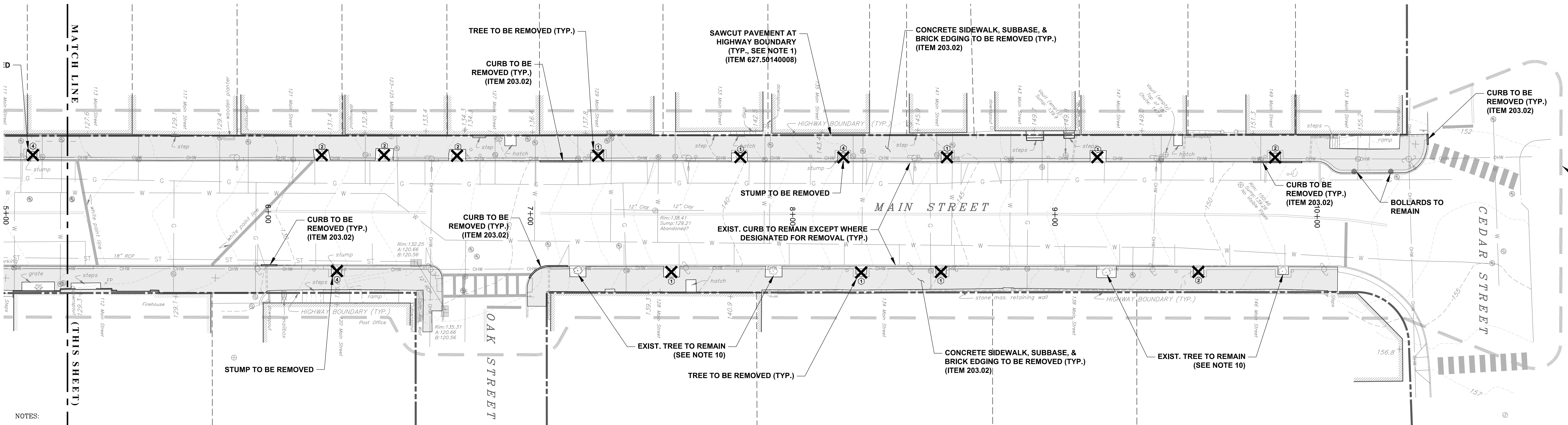
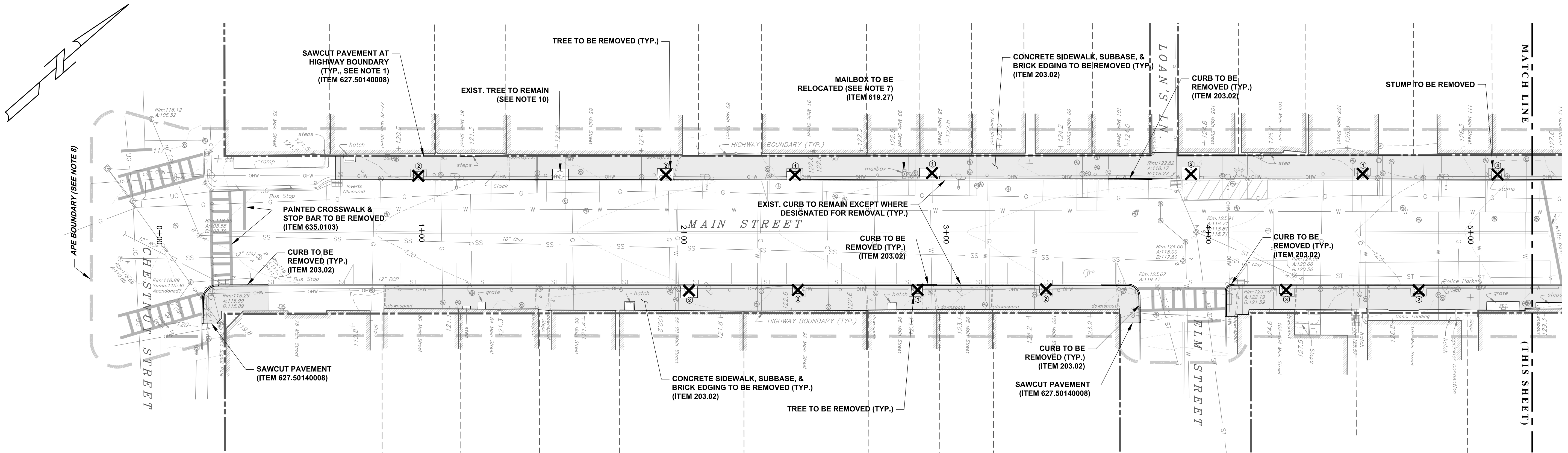
N.Y.S.D.O.T. STANDARD SHEETS REFERENCE	
ITEM	NYSDOT STANDARD SHEETS
CURB RAMP DETAILS & NOTES	SERIES 608-01
MAINTENANCE & PROTECTION OF TRAFFIC	SERIES 619

DRAWING INDEX

SHEET #	DWG. #	TITLE
1	C-1	COVER SHEET & NOTES
2	C-2	EXISTING CONDITIONS & DEMOLITION PLAN
3	C-3	LAYOUT & MATERIALS PLAN
4	C-4	SITE PLAN
5	C-5	DETAILS (SHEET 1)
6	C-6	DETAILS (SHEET 2)
7	C-7	CURB RAMP DETAILS & NOTES

VILLAGE ADMINISTRATOR, VILLAGE OF DOBBS FERRY: CHARLENE INDELICATO
DATE 8/12/19

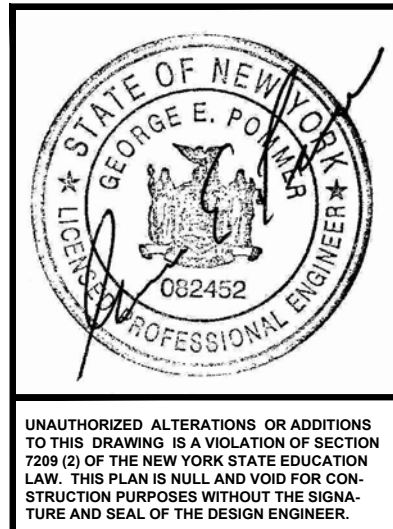
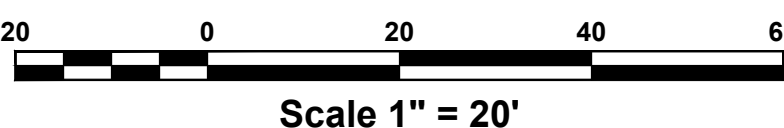
	9	1/24/22	PER NYSDOT	TITLE	COVER SHEET & NOTES
	8	1/4/22	PER NYSDOT	PROJECT	
	7	12/14/21	CROSSWALKS ADDED		
	6	11/12/21	PER VILLAGE		
	5	2/12/20	ISSUED FOR BID		
	4	12/20/20	PER NYSDOT		
	3	11/25/19	PER VILLAGE		
	2	9/9/19	PER NYSDOT		
	1	8/14/19	NYSDOT SUBMISSION		
	REV.		DATE		
VILLAGE OF DOBBS FERRY, WESTCHESTER COUNTY, NEW YORK					
UNAUTHORIZED ALTERATIONS OR ADDITIONS TO THIS DRAWING IS A VIOLATION OF SECTION 7209 (2) OF THE NEW YORK STATE EDUCATION LAW. THIS PLAN IS NULL AND VOID FOR CONSTRUCTION PURPOSES WITHOUT THE SIGNATURE AND SEAL OF THE DESIGN ENGINEER.					
		JAMES J. HAHN ENGINEERING, P.C.		Putnam Business Park 1689 Route 22 Brewster, New York 10509 Tel: (845) 279-2220	
SCALE		AS NOTED		DATE 6/6/19	
DRAWING NO.		C-1		SHEET NO. 1 OF 7	



- NOTES:
- The Highway Boundary (public right-of-way) shall be located in the field by a licensed surveyor. Existing pavement to be sawcut inside the Highway Boundary. No new construction is to take place on private property without prior documented permission from the property owner.
 - Existing sidewalk hatches and sidewalk grates fully inside of the Highway Boundary shall be removed and reset if required. Those which overlap the Highway Boundary shall remain in place unless prior documented permission has been acquired from the property owner.
 - Existing curb is to remain except as shown hereon. Additional removal and replacement of damaged curb may be required as ordered by the Engineer.
 - Existing parking meters are to be removed, stored, and replaced by Village forces. The Contractor shall install new parking meter sleeves where required.
 - Where necessary, existing signage is to be removed by the Contractor and held by the Village for storage. The Contractor shall supply and install any temporary signage required for traffic and parking control. Any new sign posts that are required shall be of like size and materials and shall be furnished and installed by the Contractor. The Contractor shall reinstall stored signage upon completion of construction.
 - Existing trash receptacles are to be removed, stored, and reinstalled by Village forces.
 - The existing mailbox shall be temporarily relocated by the Contractor to a location approved by the local postmaster and shall be returned to its original location upon completion of construction.
 - The vertical extent of the Area of Potential Effect (APE) is to 4'-0" in depth in areas of tree replacement and to the depth of the subbase replacement (varies 11' to 20') in areas of sidewalk and driveway replacement.
 - Tree and stump removal shall include grinding and removing the roots to a minimum depth of 12" below the finished grade of the sidewalk. Roots underneath curbing and the roadway surface are to remain in place.
 - Trees to remain shall be protected from soil overcompaction and root damage. The Contractor shall keep the soil around the tree moist at all times and shall not expose roots for more than 4 hours. The Engineer may require additional protection of trees to remain.

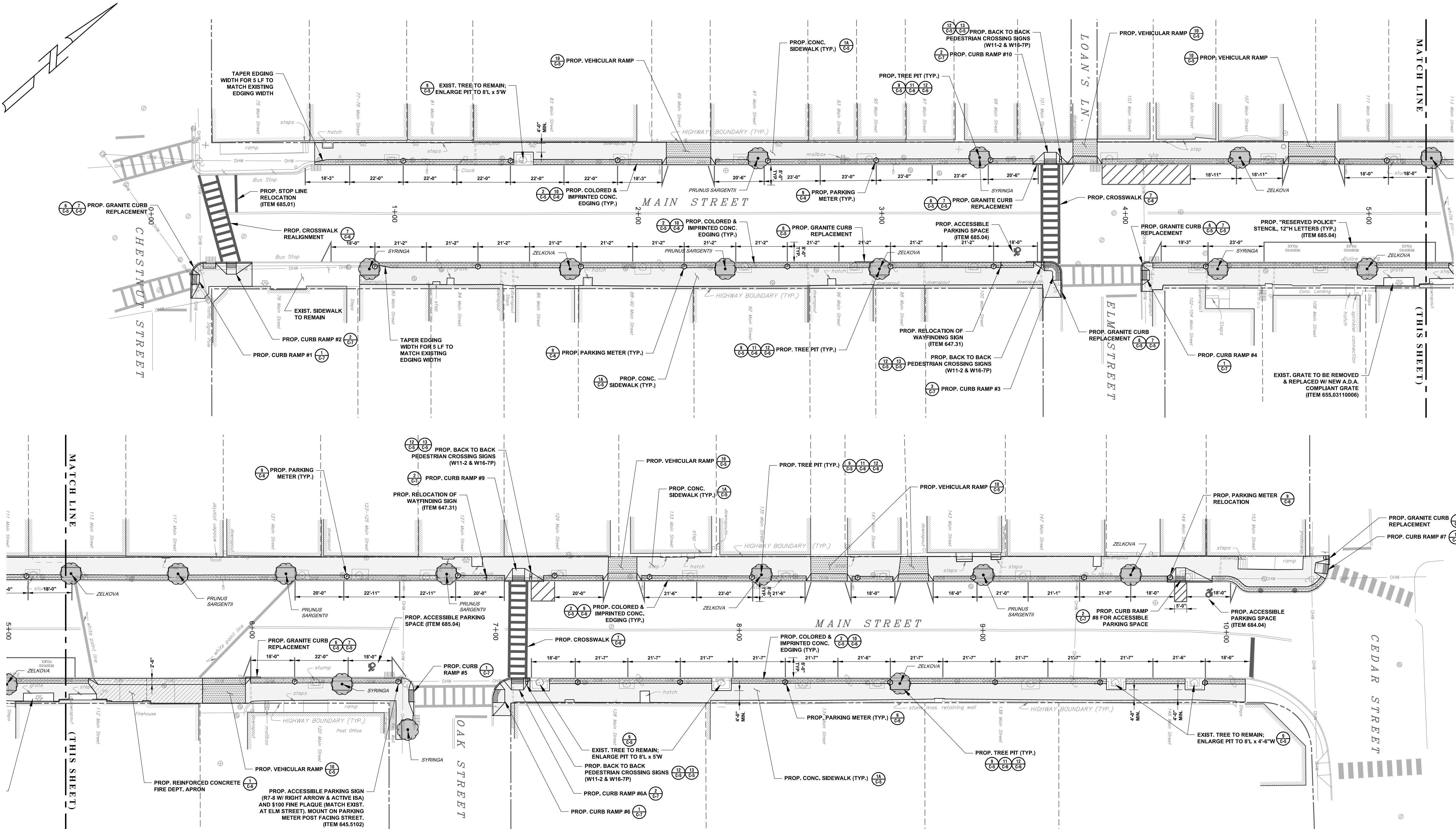
TREE REMOVAL N.Y.S.D.O.T. ITEM NUMBERS		
KEY #	ITEM NO.	CAL.
①	614.060204	6 - 12"
②	614.060304	>12" - 18"
③	614.060404	>18 - 24"
④	614.0701	STUMP

- LEGEND
- ① TREE / STUMP TO BE REMOVED & KEY #
 - CONC. SIDEWALK, BRICK EDGING, & SUBBASE TO BE REMOVED
 - AREA OF POTENTIAL EFFECT (APE) BOUNDARY



REV.	DATE	DESCRIPTION
9	1/24/22	PER NYS DOT
8	1/4/22	PER NYS DOT
7	12/14/21	CROSSWALKS ADDED
6	11/12/21	PER VILLAGE
5	2/12/20	ISSUED FOR BID
4	1/29/20	PER NYS DOT
3	11/25/19	PER VILLAGE
2	9/9/19	PER NYS DOT
1	8/14/19	NYS DOT SUBMISSION

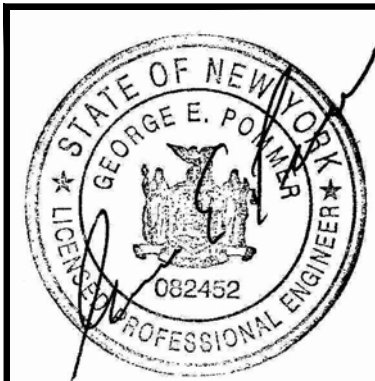
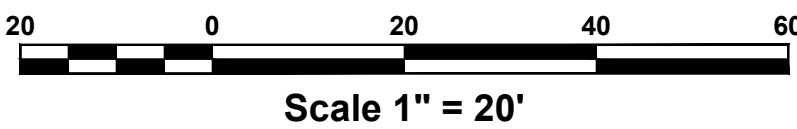
TITLE		EXISTING CONDITIONS & DEMOLITION PLAN	
PROJECT		PROPOSED MAIN STREET SIDEWALKS IMPROVEMENT PROJECT	
FEDERAL AID PROJECT NYS DOT P.I.N. 8761.52		VILLAGE OF DOBBS FERRY, WESTCHESTER COUNTY, NEW YORK	
Scale		DATE	
1" = 20'		6/6/19	
DRAWING NO.		SHEET NO.	
C-2		2 of 7	



TREE SCHEDULE					
BOTANICAL NAME	COMMON NAME	CAL.	COMMENT	ITEM	QTY.
Syringa reticulata 'Ivory Silk'	Japanese Tree Lilac	2" - 2½"	Branching at 5' height	611.0111	5
Zelkova serrata 'City Sprite'	Japanese Zelkova Wireless	1½" - 2"	Branching at 5' height	611.0111	8
Prunus sargentii 'Pink Flair'	Pink Flowering Cherry	2" - 2½"	Branching at 5' height	611.0111	6

Notes:

- Location of each tree pit and tree species to be confirmed in field with the Engineer and Village prior to ordering all planting.
- Tree selections provided by Suzanne Nolan, RLA based on August 26, 2021 Meeting with Dobbs Ferry Tree Consortium.
- Plants shall be nursery grown and shall conform to the requirements of the American Standard for Nursery Stock (ANSI Z60.1), latest edition.



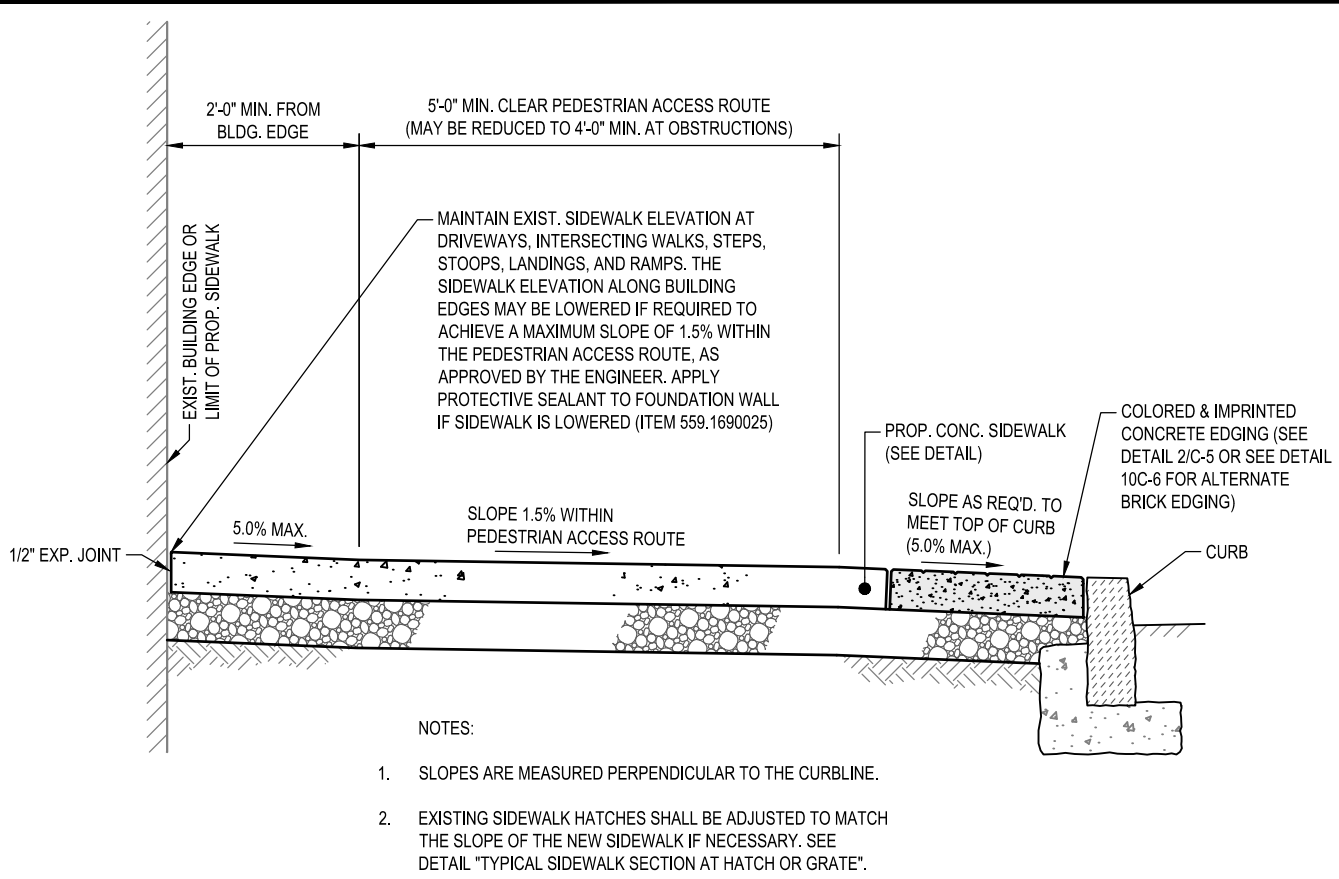
UNAUTHORIZED ALTERATIONS OR ADDITIONS TO THIS DRAWING IS A VIOLATION OF SECTION 7209 (2) OF THE NEW YORK STATE EDUCATION LAW. THIS PLAN IS NULL AND VOID FOR CONSTRUCTION PURPOSES WITHOUT THE SIGNATURE AND SEAL OF THE DESIGN ENGINEER.

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1	8/14/19	NYS DOT SUBMISSION

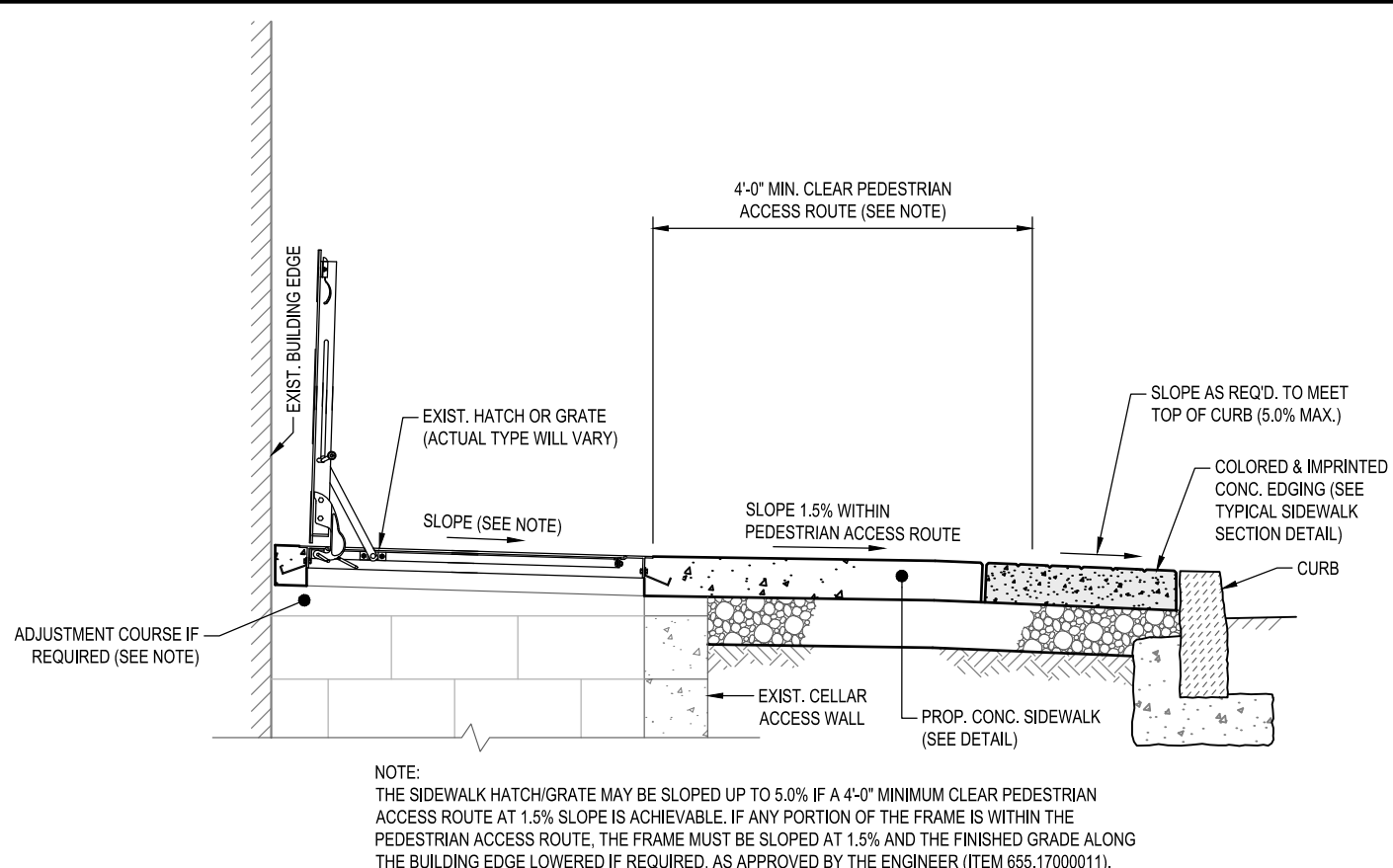
JAMES J. HAHN
ENGINEERING, P.C.

TITLE		LAYOUT & MATERIALS PLAN	
PROJECT		PROPOSED MAIN STREET SIDEWALKS IMPROVEMENT PROJECT	
FEDERAL AID PROJECT NYS DOT P.I.N. 8761.52		VILLAGE OF DOBBS FERRY, WESTCHESTER COUNTY, NEW YORK	
SCALE	DATE	SCALE	DATE
1" = 20'	6/8/19	1" = 20'	6/8/19
DRAWING NO.	SHEET NO.	DRAWING NO.	SHEET NO.
C-3	3 OF 7	C-3	3 OF 7

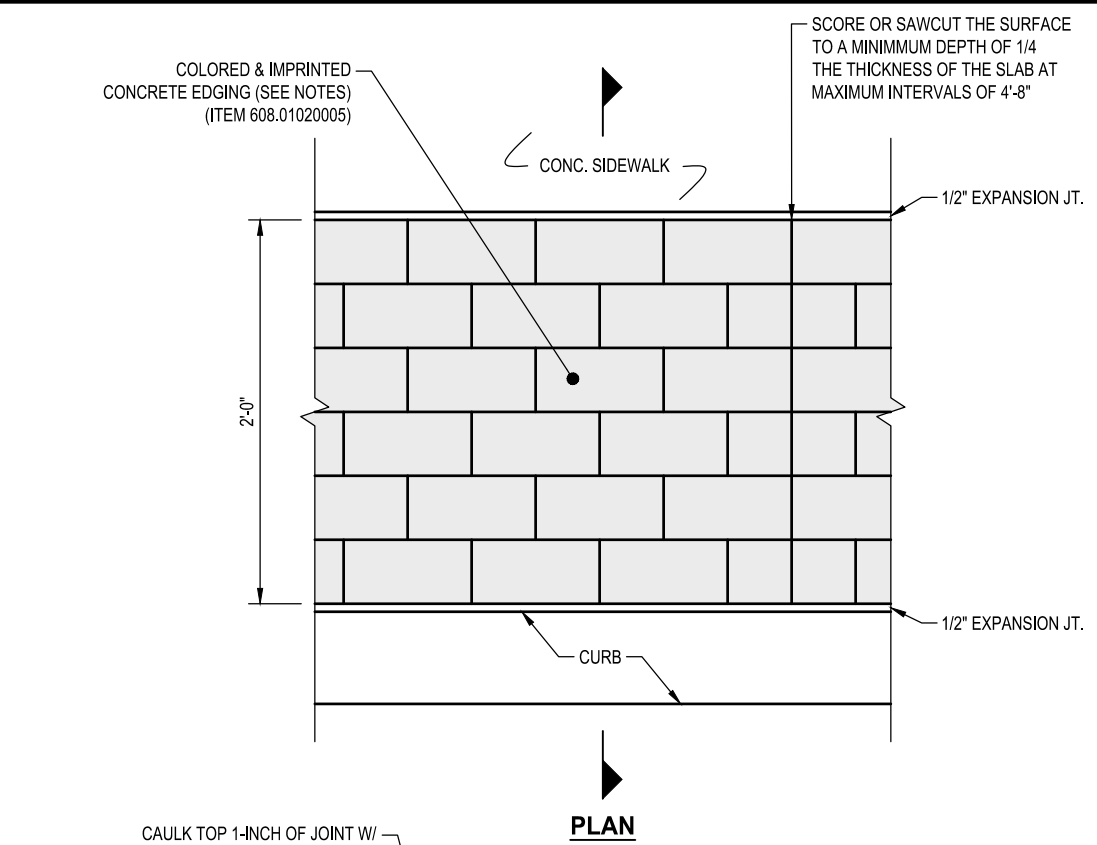
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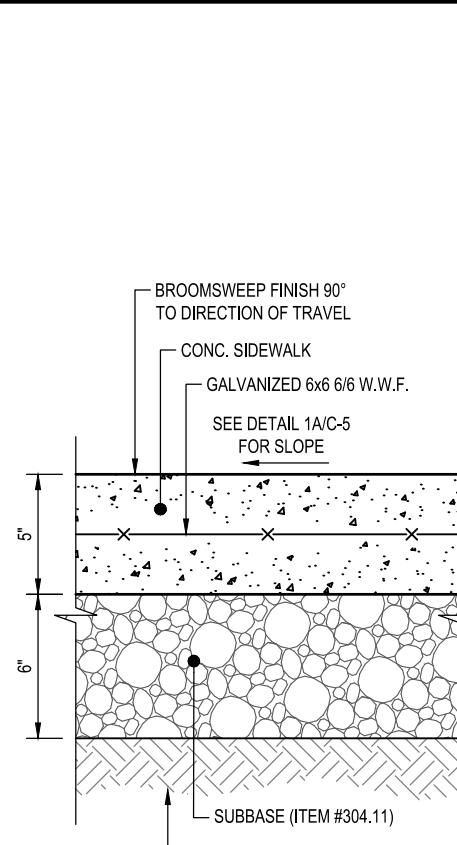
1A TYPICAL SIDEWALK SECTION
SCALE: 1/2" = 1'-0"



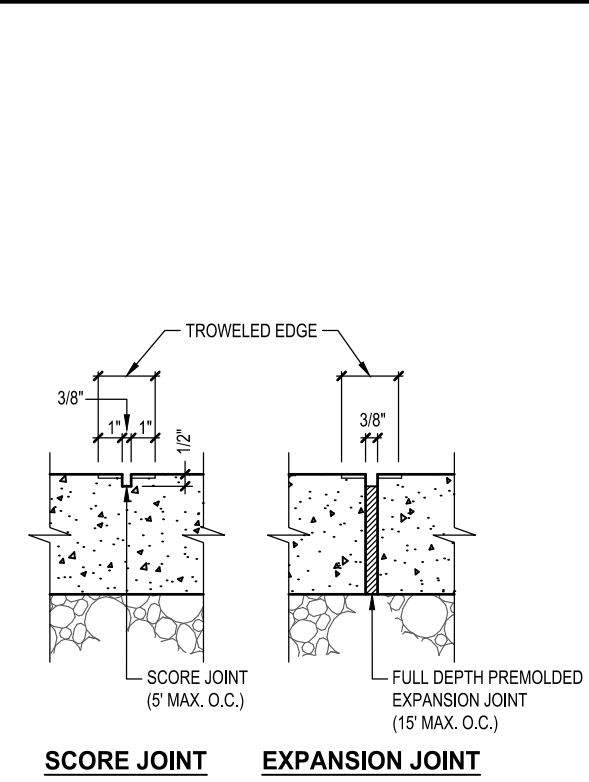
1B TYPICAL SIDEWALK SECTION AT HATCH OR GRATE
SCALE: 1/2" = 1'-0"



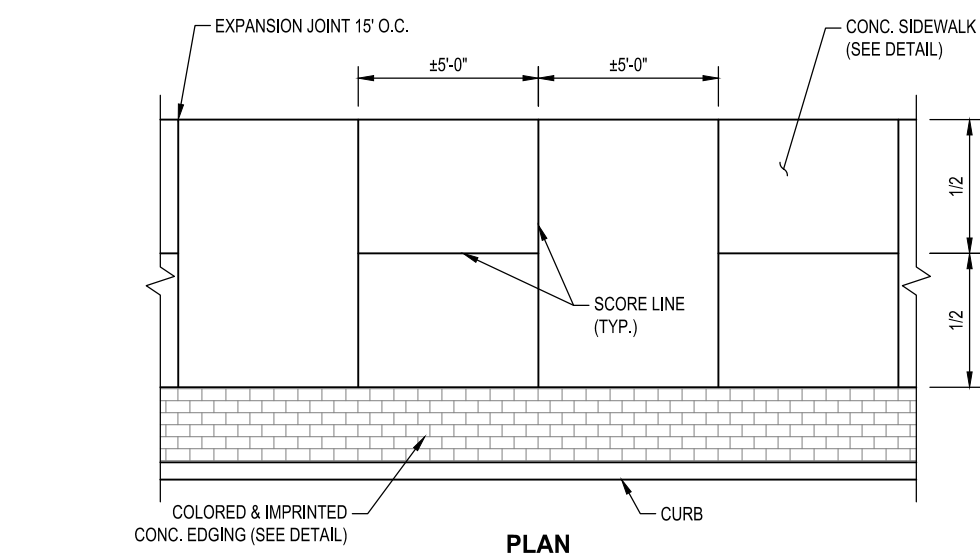
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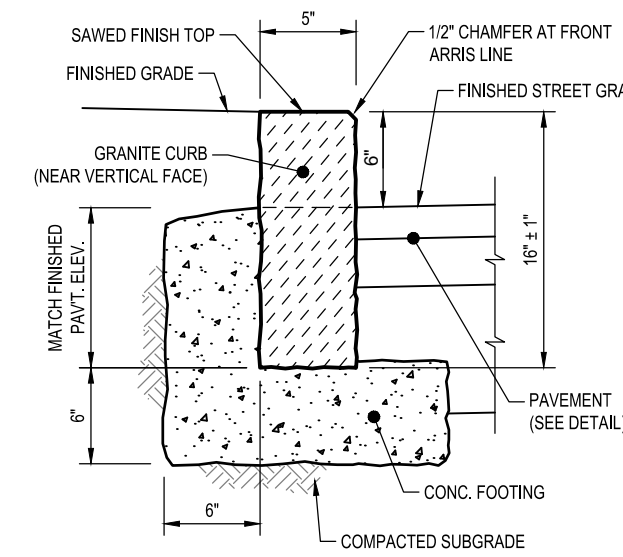
3 CONCRETE SIDEWALK
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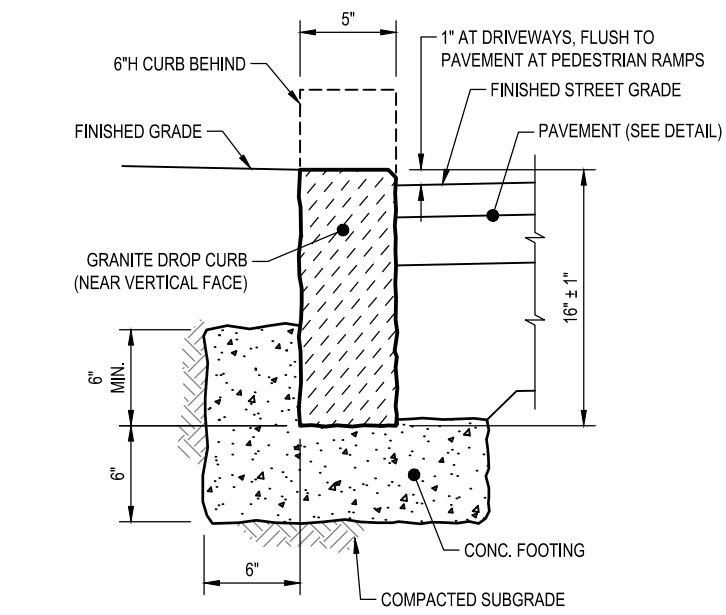
4 CONCRETE SIDEWALK AT VEHICULAR RAMP
SCALE: N.T.S.



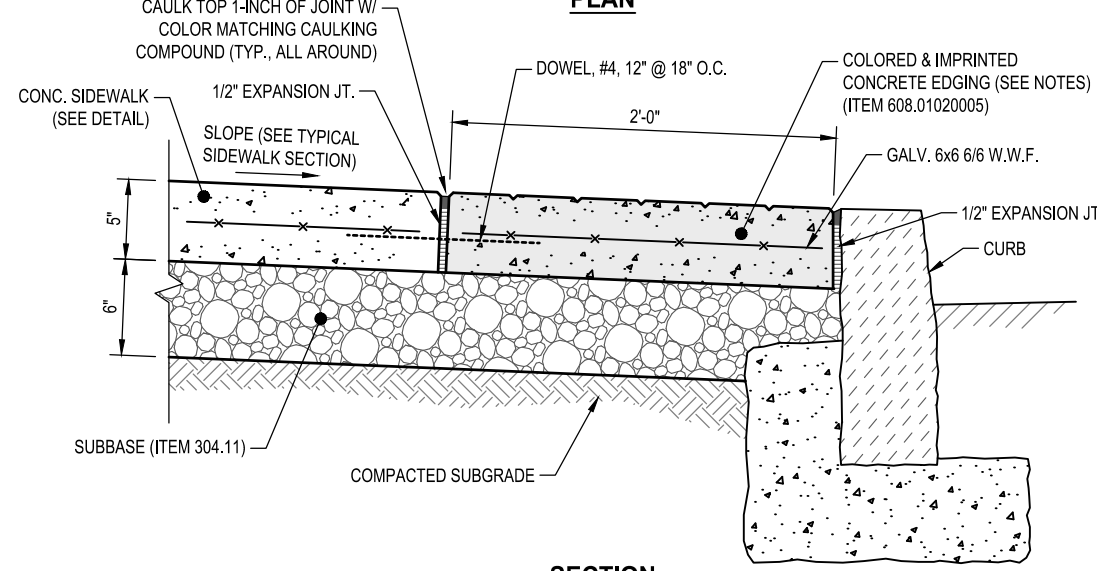
5 TYPICAL SIDEWALK SCORING PATTERN
SCALE: 3/16" = 1'-0"



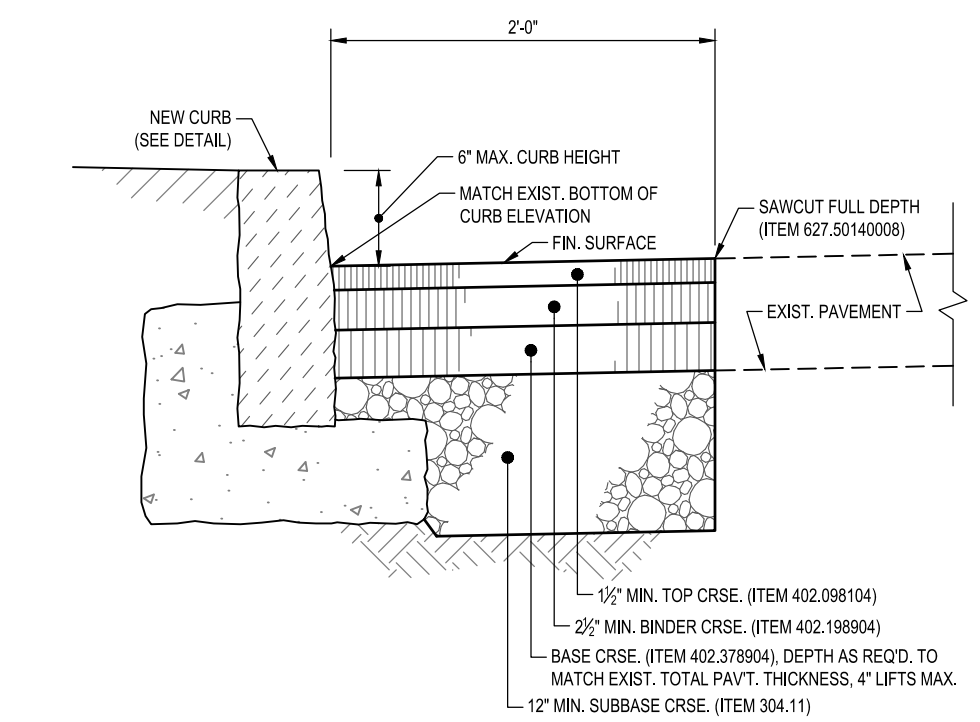
6 GRANITE CURB
SCALE: N.T.S.



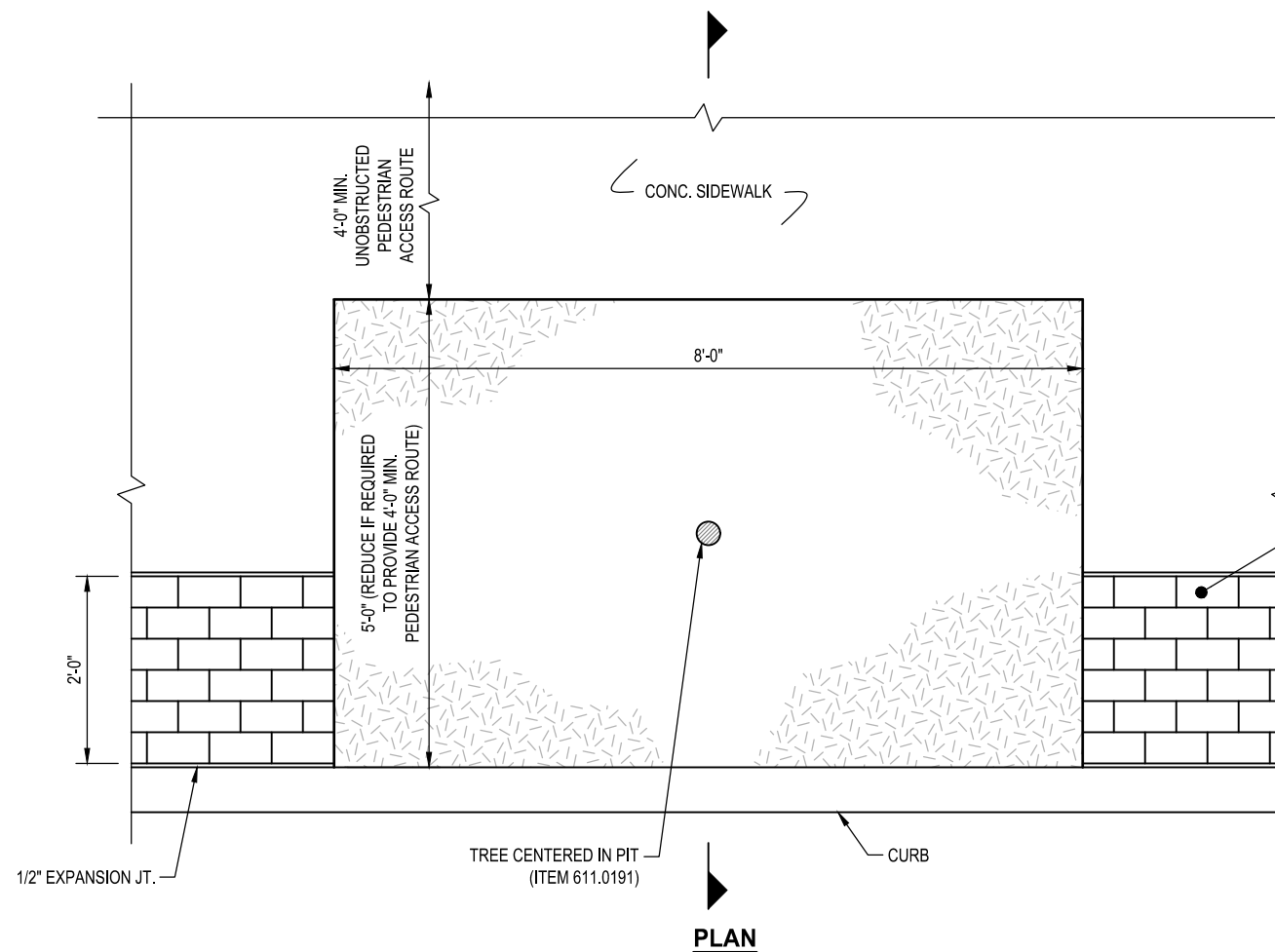
7 GRANITE DROP CURB
SCALE: N.T.S.



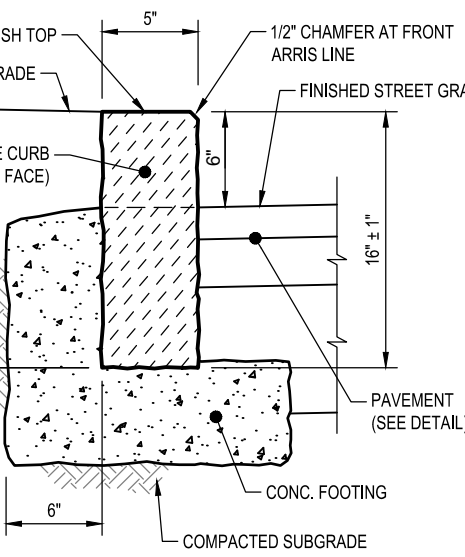
2 COLORED & IMPRINTED CONCRETE EDGING (BASE BID)
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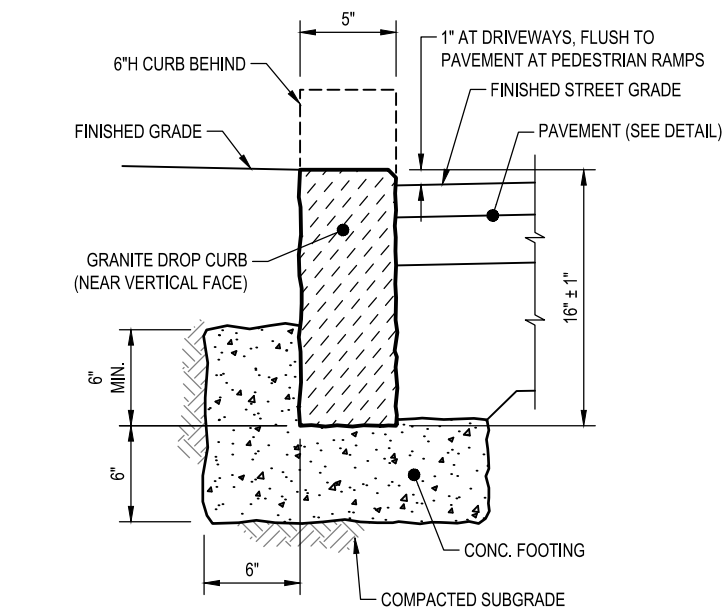
8 PAVEMENT RESTORATION AT NEW CURB
SCALE: 1" = 1'-0"



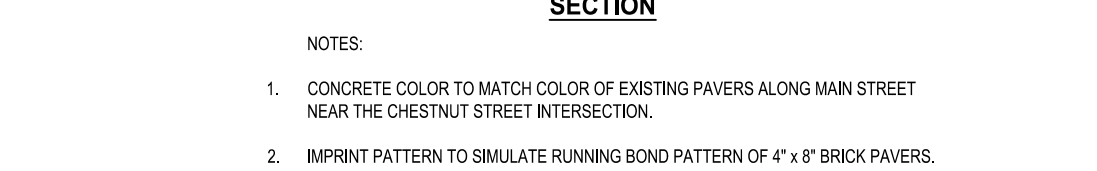
9 TREE PIT (BASE BID)
SCALE: 1/2" = 1'-0"



6 GRANITE CURB
SCALE: N.T.S.



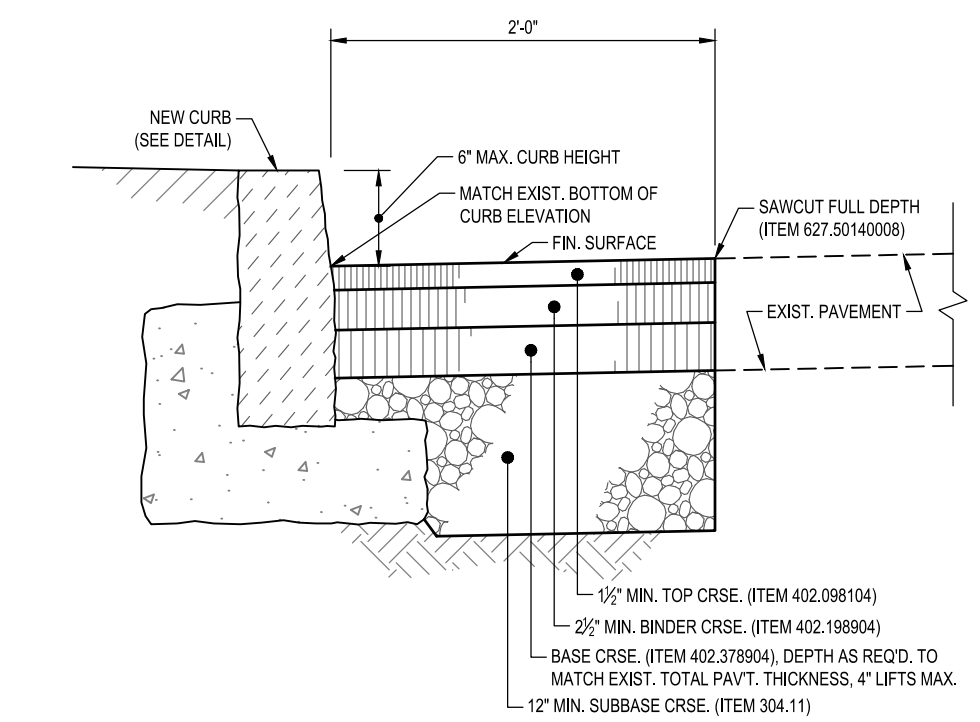
7 GRANITE DROP CURB
SCALE: N.T.S.



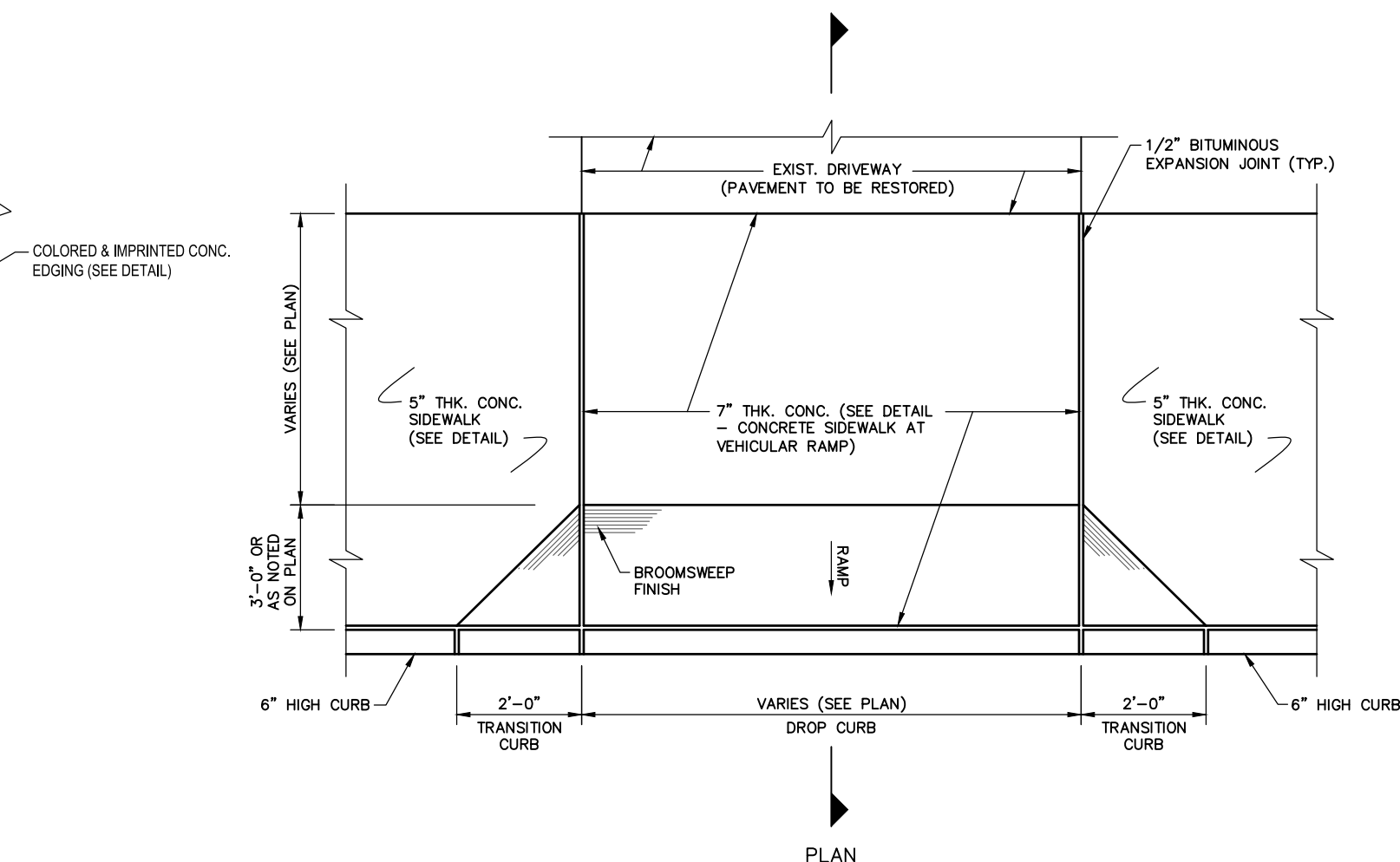
2 COLORED & IMPRINTED CONCRETE EDGING (BASE BID)
SCALE: 1" = 1'-0"



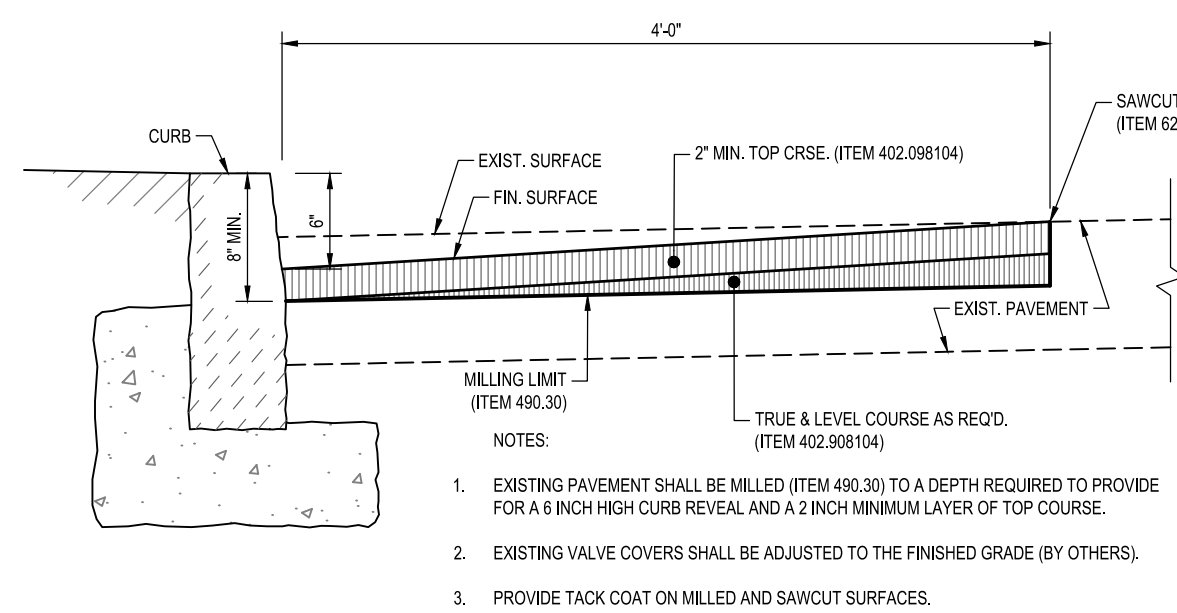
3 CONCRETE SIDEWALK
SCALE: N.T.S.



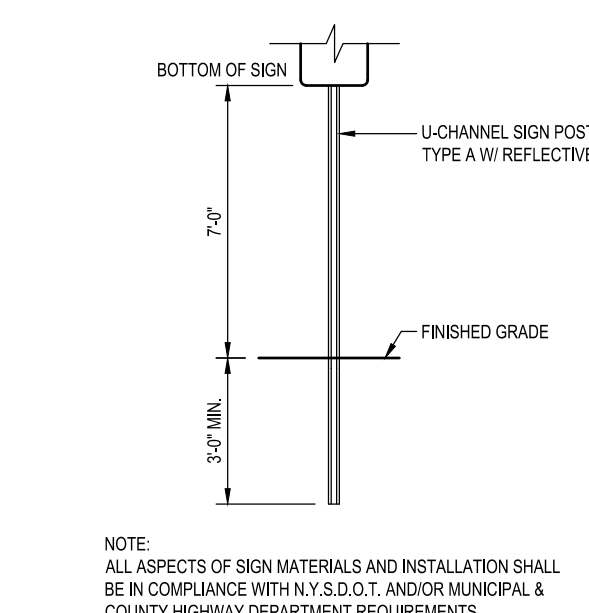
8 PAVEMENT RESTORATION AT NEW CURB
SCALE: 1" = 1'-0"



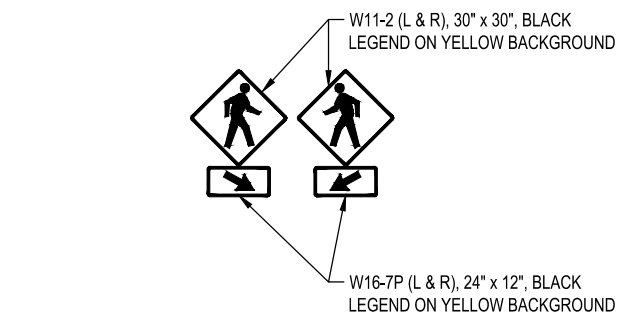
10 VEHICULAR RAMP
SCALE: N.T.S.



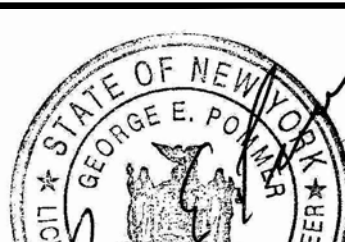

11 PAVEMENT MILLING & REPLACEMENT
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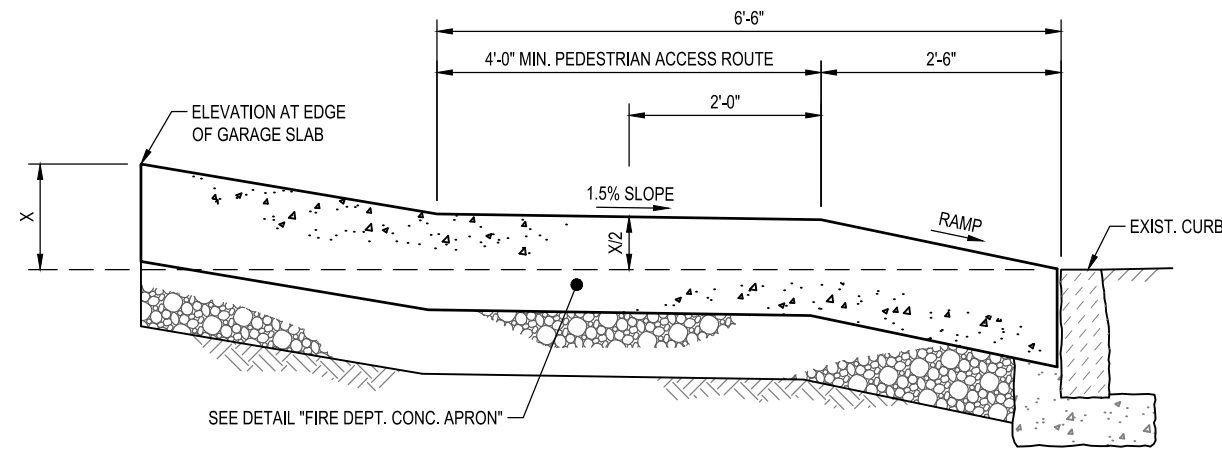


12 TRAFFIC SIGN POST
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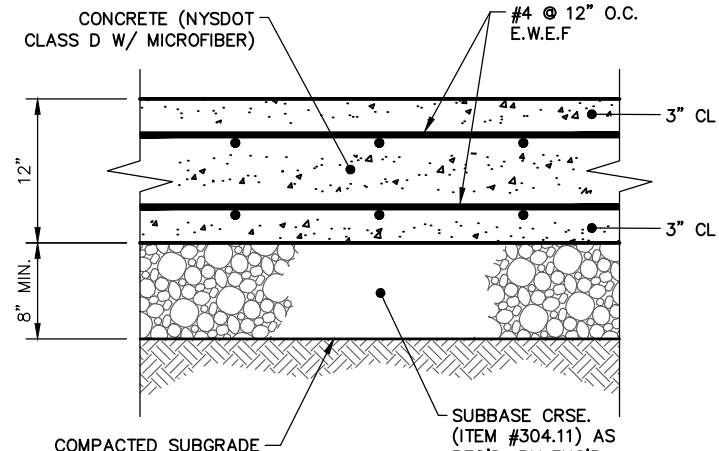


13 PEDESTRIAN CROSSWALK SIGN LEGEND
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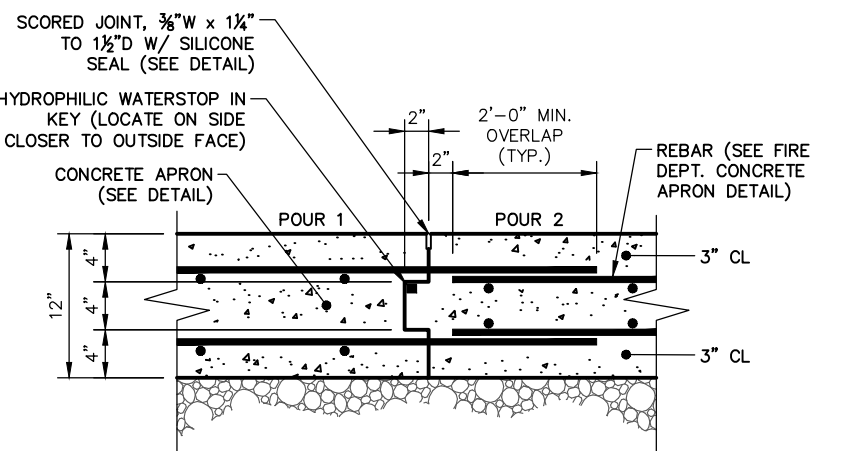
	9	1/24/22	PER NYS DOT	TITLE PROJECT	DETAILS (SHEET 1)				
	8	1/24/22	PER NYS DOT		PROPOSED MAIN STREET SIDEWALKS IMPROVEMENT PROJECT				
	7	12/14/21	CROSSWALKS ADDED						
	6	11/12/21	PER VILLAGE						
	5	2/12/20	ISSUED FOR BID						
	4	1/29/20	PER NYS DOT						
	3	11/25/19	PER VILLAGE						
	2	9/9/19	PER NYS DOT						
	1	8/14/19	NYS DOT SUBMISSION						
	REV.	DATE	DESCRIPTION			FEDERAL AID PROJECT NYS DOT P.I.N. 8761.52			
VILLAGE OF DOBBS FERRY, WESTCHESTER COUNTY, NEW YORK									
UNAUTHORIZED ALTERATIONS OR ADDITIONS TO THIS DRAWING IS A VIOLATION OF SECTION 7209 (2) OF THE NEW YORK STATE EDUCATION LAW. THIS PLAN IS NULL AND VOID FOR CONSTRUCTION PURPOSES WITHOUT THE SIGNATURE AND SEAL OF THE DESIGN ENGINEER.									
		JAMES J. HAHN ENGINEERING, P.C.		Putnam Business Park 1689 Route 22 Brewster, New York 10809 Tel: (845) 279-2220		SCALE AS NOTED DRAWING NO. C-5		DATE 6/6/19 SHEET NO. 5 OF 7	



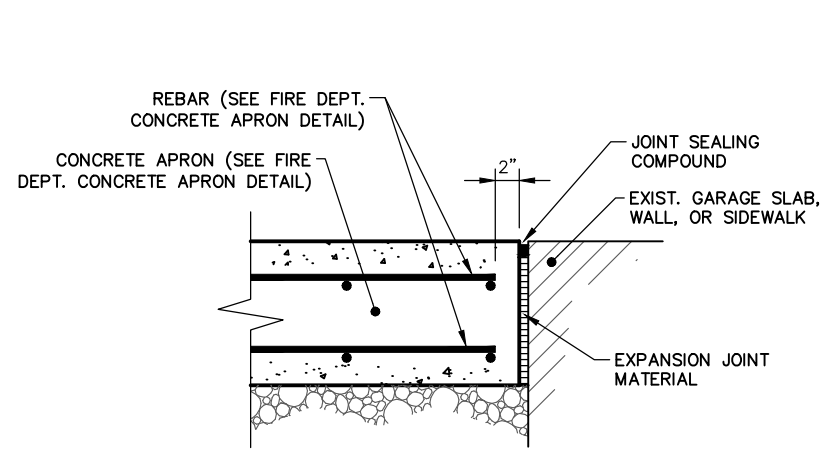
1 TYPICAL FIRE DEPT. CONCRETE APRON PROFILE
SCALE: 1/2" = 1'-0"



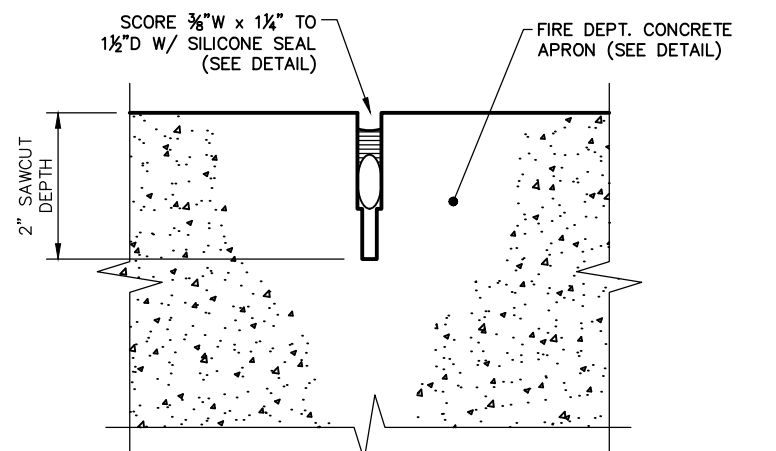
2 FIRE DEPT. CONCRETE APRON
(ITEM 608.0101) N.T.S.



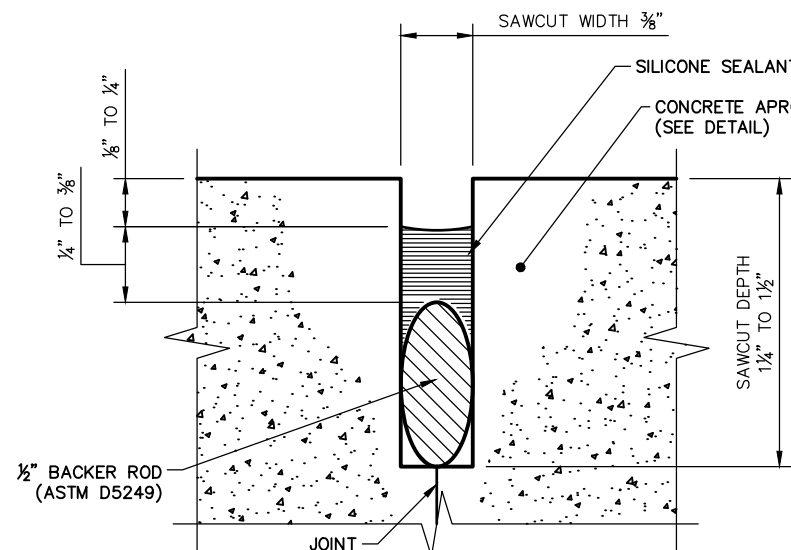
3 FIRE DEPT. APRON CONSTRUCTION JOINT
(ITEM 608.0101) N.T.S.



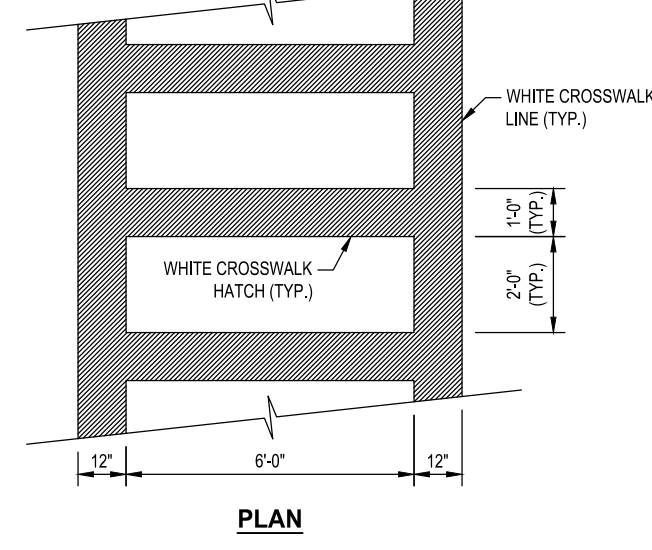
4 FIRE DEPT. APRON EXPANSION JOINT
(ITEM 608.0101) N.T.S.



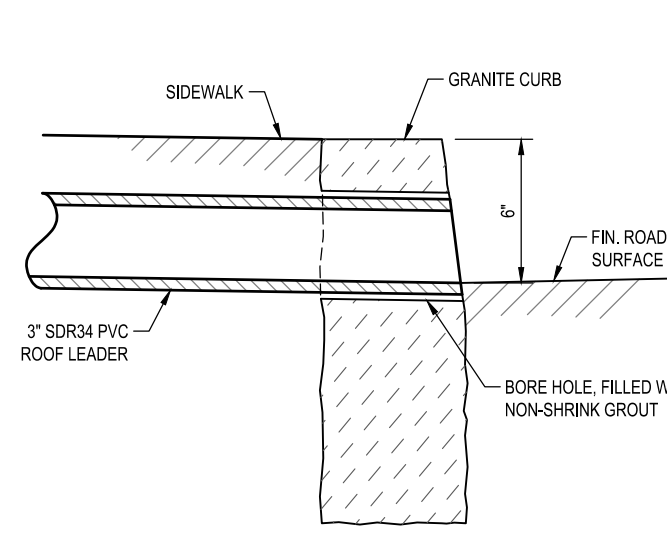
5 CONTROL JOINT
(ITEM 608.0101) N.T.S.



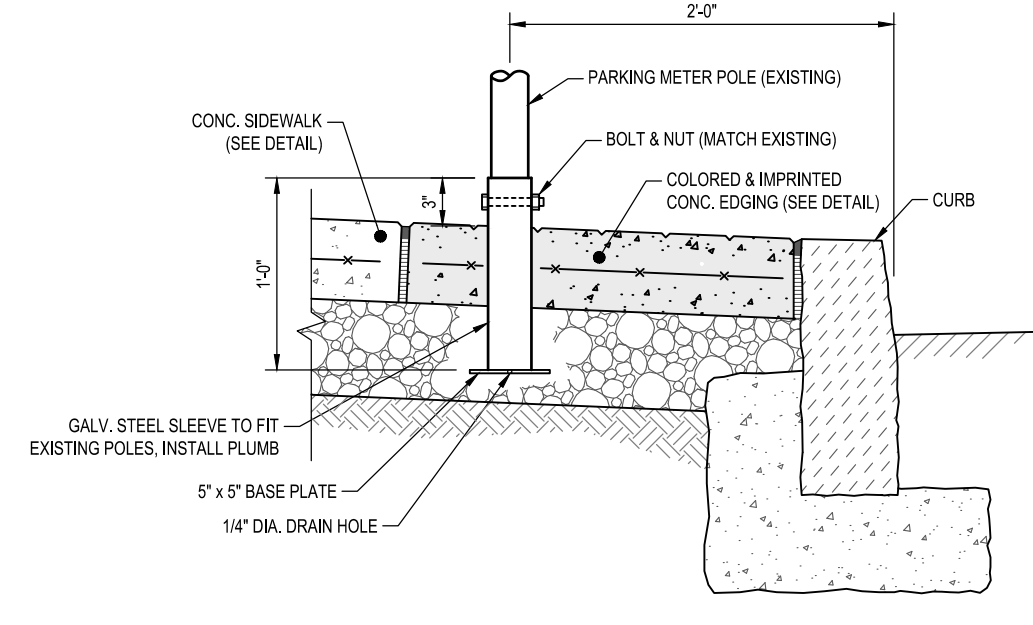
6 SILICONE SEAL
(ITEM 608.0101) N.T.S.



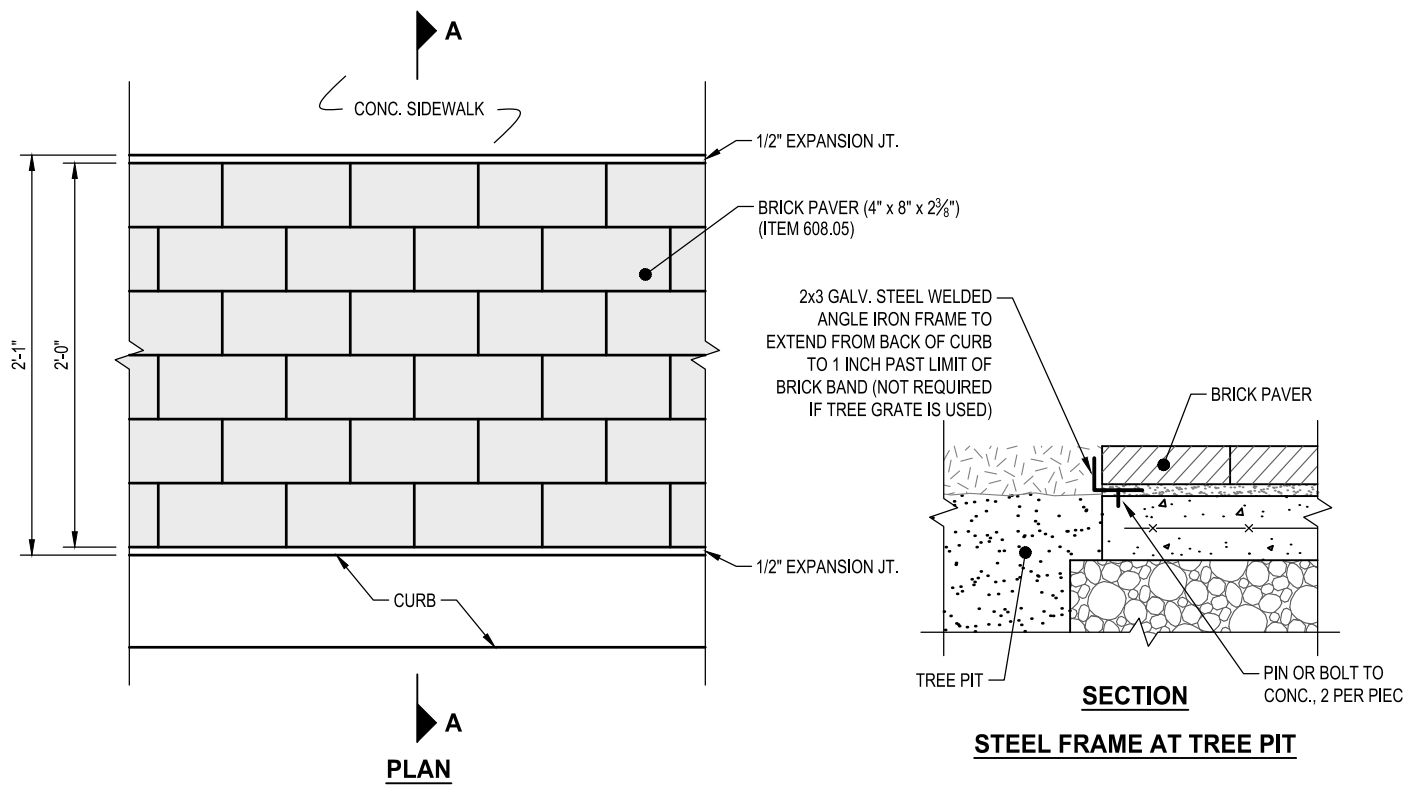
7 CROSSWALK PAVEMENT MARKING (TYPE LS)
(ITEM 685.01) N.T.S.



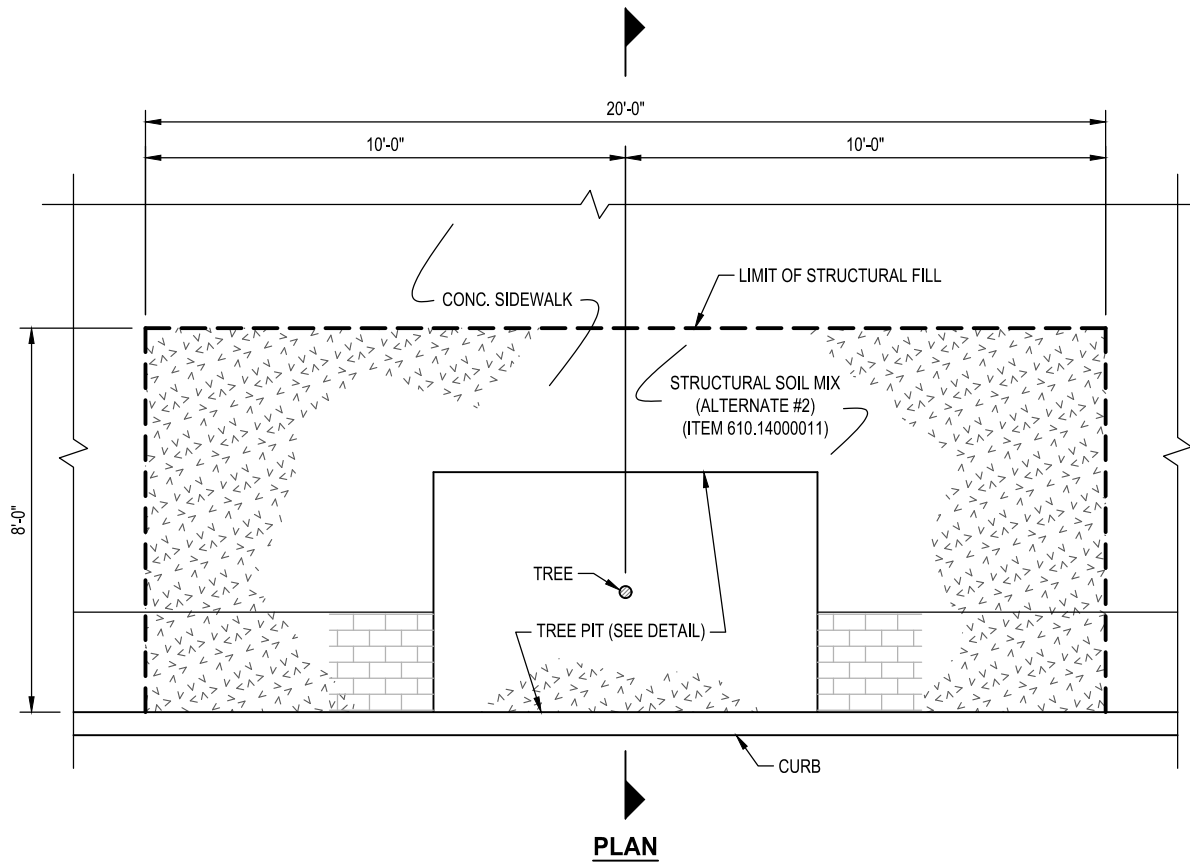
8 ROOF LEADER DISCHARGE THRU CURB
(ITEM 576.2001) N.T.S.



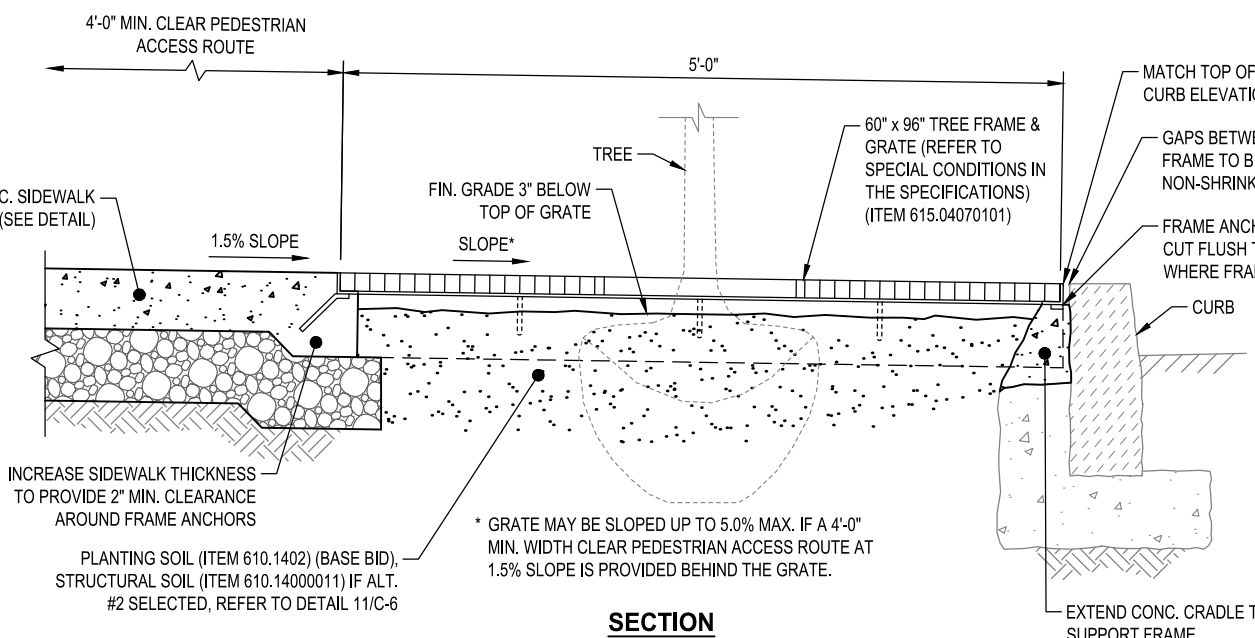
9 PARKING METER SLEEVE
(ITEM 634.06010007) SCALE: 1\"/>



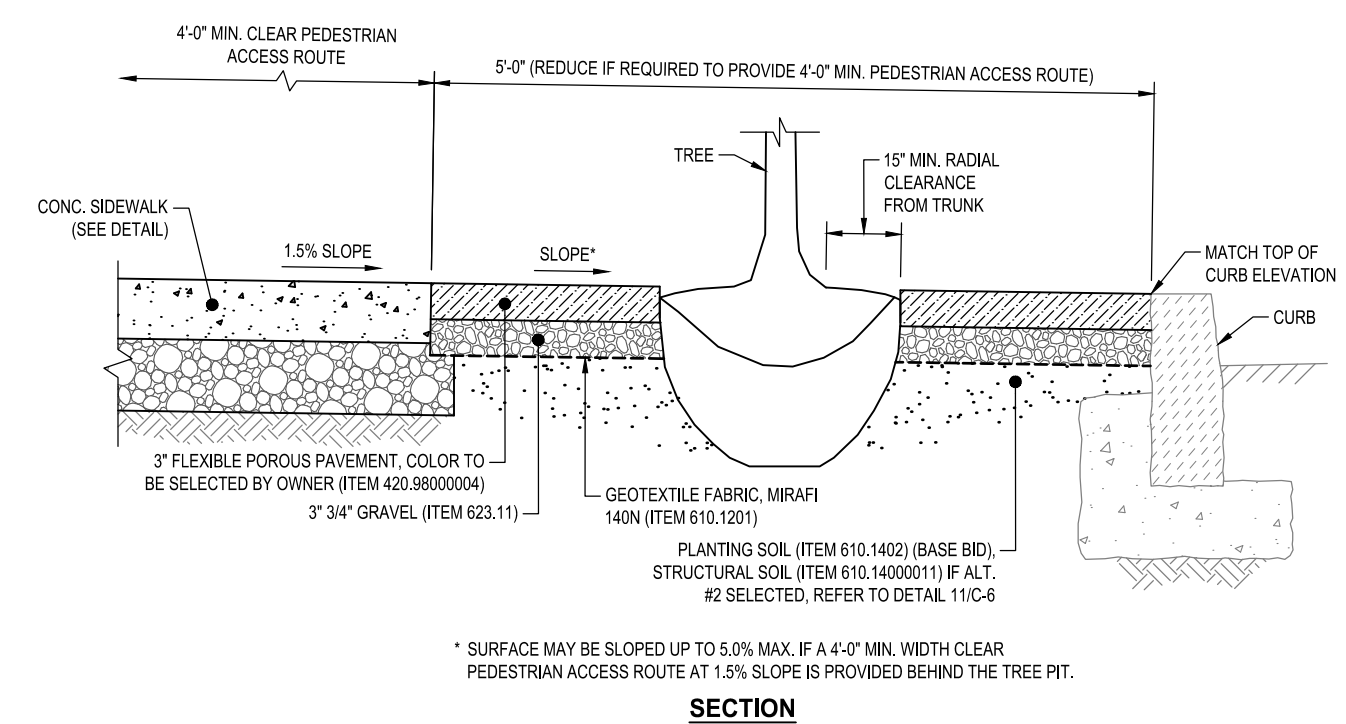
10 BRICK PAVER EDGING (ALTERNATE #4)
SCALE: 1\"/>



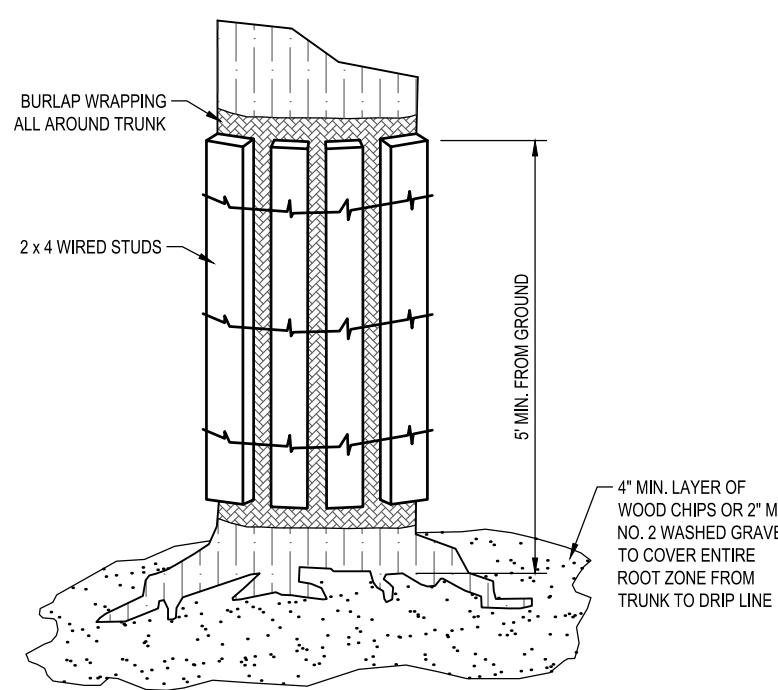
11 TREE PIT W/ STRUCTURAL SOIL MIX (ALTERNATE #2)
N.T.S.



12 TREE GRATE (ALTERNATE #3)
SCALE: 3/4\"/>



13 TREE PIT WITH FLEXIBLE POROUS PAVEMENT (ALTERNATE #1)
SCALE: 3/4\"/>



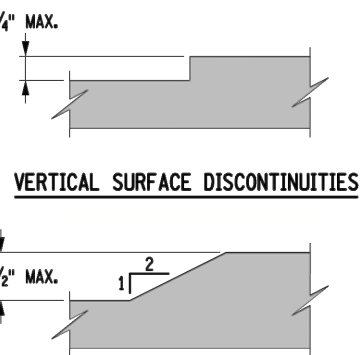
- NOTES:
- NO MATERIALS SHALL BE NAILED TO THE PROTECTED TREE.
 - TREE ARMORING IS A NON-PAYMENT ITEM.

14 TREE ARMORING
N.T.S.

TITLE				DETAILS (SHEET 2)			
PROJECT				PROPOSED MAIN STREET SIDEWALKS IMPROVEMENT PROJECT			
FEDERAL AID PROJECT NYSDOT P.I.N. 8761.52				VILLAGE OF DOBBS FERRY, WESTCHESTER COUNTY, NEW YORK			
REV. DATE DESCRIPTION				SCALE AS NOTED DATE 6/6/19			
9	1/24/22	PER NYSDOT		C-6 6 of 7			
8	1/4/22	PER NYSDOT					
7	12/14/21	CROSSWALKS ADDED					
6	11/12/21	PER VILLAGE					
5	2/12/20	ISSUED FOR BID					
4	1/29/20	PER NYSDOT					
3	11/25/19	PER VILLAGE					
2	9/9/19	PER NYSDOT					
1	8/14/19	NYSDOT SUBMISSION					

GENERAL NOTES:

- THESE SHEETS ARE IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT (ADA), AND THE REQUIREMENTS OF THE 2011 PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT OF WAY (PROWAG).
- THE DIMENSIONS SHOWN IN THE DETAILS AS MINIMUMS AND MAXIMUMS ARE THE LIMITS FOR DESIGN AND FIELD LAYOUT. FOR WORK ACCEPTANCE VALUES SEE "CRITICAL ELEMENTS FOR THE DESIGN, LAYOUT, AND ACCEPTANCE OF PEDESTRIAN FACILITIES" ON STANDARD SHEETS 608-01, 11 OF 12 & 12 OF 12.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD VERIFYING ALL ELEVATIONS AND DIMENSIONS TO ENSURE THAT THE FINAL LAYOUT OF PEDESTRIAN FACILITIES MEETS ADA REQUIREMENTS. ANY SURVEY WORK NECESSARY TO MEET THESE REQUIREMENTS SHALL BE PAID FOR UNDER ITEM 625.01 - SURVEY OPERATIONS.
- FACILITIES THAT CANNOT BE CONSTRUCTED TO MEET THE DESIGN STANDARDS, DUE TO DESIGN CONSTRAINTS, SHALL BE NOTED WITH THE "NOTES ON INSPECTION METHODS (MEASUREMENT)" ON STANDARD SHEET 608-01, 11 OF 12.
- TO CHECK FIELD LAYOUT AND TO VERIFY WORK ACCEPTANCE, ALL MEASUREMENTS SHALL BE MADE IN ACCORDANCE WITH THE "NOTES ON INSPECTION METHODS (MEASUREMENT)" ON STANDARD SHEET 608-01, 11 OF 12.
- JOINTS BETWEEN SIDEWALKS, CURB RAMPS, TURNING SPACES AND ROADWAYS SHALL BE FLUSH AND FREE FROM ABRUPT VERTICAL CHANGES GREATER THAN 1/4". VERTICAL SURFACE DISCONTINUITIES BETWEEN 1/4" AND 1/2" SHALL BE BEVELED WITH A SLOPE NOT STEEPER THAN 1:2. THE BEVEL SHALL BE APPLIED ACROSS THE ENTIRE JOINT. SEE "VERTICAL SURFACE DISCONTINUITIES" DETAIL ON SHEET C-7.
- SIDEWALKS ARE CONNECTED TO ROADWAYS BY BLENDED TRANSITIONS OR CURB RAMPS. BLENDED TRANSITIONS ARE CONNECTIONS BETWEEN THE SIDEWALK LEVEL AND THE ROADWAY LEVEL THAT HAVE A MAXIMUM GRADE (RUNNING SLOPE) OF 5%. CONNECTIONS WITH A MAXIMUM GRADE (RUNNING SLOPE) GREATER THAN 5% ARE CONSIDERED CURB RAMPS.
- CURB RAMPS AND BLENDED TRANSITIONS MAY REQUIRE THE INSTALLATION OF DETECTABLE WARNINGS. SEE ADDITIONAL "DETECTABLE WARNING NOTES" ON THIS SHEET, AND THE DETAILS ON SHEET C-7 FOR DIMENSIONS AND ORIENTATION.
- GRADE BREAKS WITHIN THE PEDESTRIAN ACCESS ROUTE SHOULD BE PERPENDICULAR TO THE DIRECTION OF TRAVEL AND SHALL NOT BE ROUNDED. VERTICAL ALIGNMENT SHALL BE GENERALLY PLANAR.
- MATERIAL DEPTHS SHOWN ON THESE SHEETS ARE TYPICAL MINIMUM VALUES AND MAY BE DIFFERENT IN THE CONTRACT DOCUMENTS.
- SIDEWALK GRADE (RUNNING SLOPE) SHALL NOT EXCEED 4.5% FOR DESIGN AND LAYOUT OR 5% FOR WORK ACCEPTANCE, EXCEPT WHEN MATCHING INTO EXISTING SIDEWALK OR WHEN THE ADJACENT HIGHWAY GRADE IS STEEPER THAN 5%. WHEN THE ADJACENT HIGHWAY GRADE IS GREATER THAN 5%, THE SIDEWALK GRADE SHALL NOT EXCEED THE HIGHWAY GRADE.
- THE CROSS SLOPE OF PEDESTRIAN ACCESS ROUTES SHALL BE 1.5% MAXIMUM FOR DESIGN AND LAYOUT, AND 2% MAXIMUM FOR WORK ACCEPTANCE. THE FOLLOWING EXCEPTIONS ARE ALLOWED:
 - WHERE PEDESTRIAN STREET CROSSINGS ARE PROVIDED AT INTERSECTIONS WITHOUT YIELD- OR STOP-CONTROL, OR WHERE THERE IS ANY TRAFFIC SIGNAL WITHOUT A FLASHING RED, THE CROSS SLOPE OF A PEDESTRIAN ACCESS ROUTE CONTAINED WITHIN A STREET CROSSING SHALL BE 4.5% MAXIMUM FOR DESIGN AND LAYOUT, AND 5% MAXIMUM FOR WORK ACCEPTANCE.
 - WHERE MIDBLOCK PEDESTRIAN STREET CROSSINGS ARE PROVIDED, THE CROSS SLOPE OF A PEDESTRIAN ACCESS ROUTE CONTAINED WITHIN A MIDBLOCK STREET CROSSING SHALL BE PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE.
- THE MINIMUM CLEAR WIDTH FOR PEDESTRIAN ACCESS ROUTES IS 4'-0". EXCLUSIVE OF THE CURB, THE DEPARTMENT'S PREFERRED CLEAR WIDTHS ARE LESS THAN 5'-0", 5'-0" x 5'-0" PASSING SPACES, OR A FEATURE OF EQUAL OR GREATER DIMENSIONS THAT MEETS THE SLOPE AND SURFACE CRITERIA, SHALL BE PROVIDED AT A MAXIMUM INTERVAL OF 200'. EXISTING DRIVEWAYS AND STREET CROSSINGS MAY SERVE AS PASSING SPACES, PROVIDED THEY MEET SLOPE AND SURFACE REQUIREMENTS FOR A PEDESTRIAN ACCESS ROUTE.
- THE BUFFER ZONE IS A PHYSICAL DISTANCE SEPARATING THE PEDESTRIAN ACCESS ROUTE FROM THE VEHICLE TRAVELED WAY. THE BUFFER ZONE MAY BE PLANTED OR PAVED. WHERE THE BUFFER ZONE WIDTH, EXCLUSIVE OF CURB, IS LESS THAN 3'-0", THE SURFACE SHOULD BE PAVED OR CONSTRUCTED WITH HARDSCAPE MATERIALS.
- THE MAXIMUM RECOMMENDED CROSS SLOPE OF A TURF BUFFER ZONE OR SLOPE TRANSITION BEHIND SIDEWALK IS 25%. BUFFER ZONES WITH A CROSS SLOPE GREATER THAN 25% SHOULD BE PAVED, PLANTED OR CONSTRUCTED WITH HARDSCAPE MATERIALS.
- WHEN CROSSING DRIVEWAYS, THE WORK SHALL BE IN CONFORMANCE WITH STANDARD SHEET 608-03.
- FOR PEDESTRIAN SIGNALS AND PEDESTRIAN PUSH BUTTONS, REFER TO STANDARD SHEET 608-01, 12 OF 12 AND STANDARD SHEET 680-10 FOR DETAILS.
- WHERE EXISTING ROADWAYS ARE SAWCUT TO INSTALL CURBING AND/OR SIDEWALK, THE ROADWAY SHOULD BE SAWCUT AT LEAST 2'-0" FROM THE PROPOSED CURB LINE TO ALLOW FOR ADEQUATE COMPACTION OF ASPHALT. IF THE SAWCUT IS LESS THAN 2'-0" FROM THE PROPOSED CURB LINE, THE ROADWAY SHALL BE REBUILT USING CLASS A, C, OR D CONCRETE.



VERTICAL SURFACE DISCONTINUITIES

CURB RAMP NOTES:

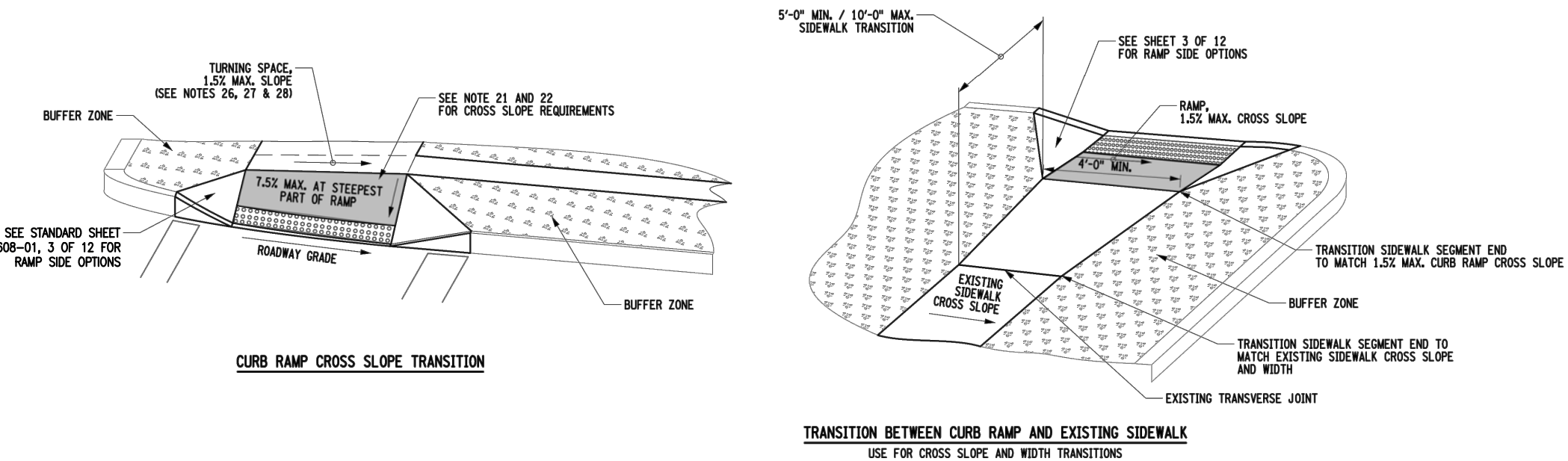
- THE MINIMUM CLEAR WIDTH OF A CURB RAMP SHALL BE 4'-0". THE DEPARTMENT'S PREFERRED CLEAR WIDTH IS 5'-0".
- THE MAXIMUM GRADE (RUNNING SLOPE) FOR DESIGN AND LAYOUT OF A CURB RAMP SHALL BE 7.5%. THE GRADE FOR WORK ACCEPTANCE SHALL BE A MAXIMUM OF 8.3%.
- WHERE THE TERRAIN DOES NOT ALLOW CONSTRUCTION OF A CURB RAMP WITH A GRADE (RUNNING SLOPE) OF 8.3% OR LESS WITHIN 15'-0", THE RAMP LENGTH SHALL NOT BE REQUIRED TO EXCEED 15'-1" FOR DESIGN AND LAYOUT OR 15'-0" FOR WORK ACCEPTANCE.
- THE CROSS SLOPE OF THE CURB RAMP SHALL BE AS FLAT AS POSSIBLE AND STILL PROVIDE POSITIVE DRAINAGE. THE CROSS SLOPE OF A CURB RAMP SHALL BE 1.5% MAXIMUM FOR DESIGN AND LAYOUT, AND 2% MAXIMUM FOR WORK ACCEPTANCE. THE FOLLOWING EXCEPTIONS ARE ALLOWED:
 - WHERE PEDESTRIAN STREET CROSSINGS ARE PROVIDED AT INTERSECTIONS WITHOUT YIELD- OR STOP-CONTROL, WHERE THERE IS ANY TRAFFIC SIGNAL WITHOUT A FLASHING RED, OR AT MIDBLOCK CROSSINGS, THE CROSS SLOPE OF THE CURB RAMP SHALL BE PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE.
- WHERE THE EXISTING ROADWAY GRADE EXCEEDS THE MAXIMUM ALLOWABLE CROSS SLOPE FOR A CURB RAMP, AND CANNOT BE CORRECTED WITHIN THE SCOPE OF THE PROJECT, THE RAMP SHOULD BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE "CURB RAMP CROSS SLOPE TRANSITION" DETAIL ON SHEET C-7. THE RAMP MAY NEED TO BE JUSTIFIED AS A NONSTANDARD FEATURE. SEE NOTE 3 ON THIS SHEET.
- RAMP SIDE OPTIONS ARE DETAILED ON SHEET C-7. WHERE A PEDESTRIAN CIRCULATION PATH CROSSES THE CURB RAMP, FLARED SIDES SHALL BE INSTALLED WITH A MAXIMUM SLOPE OF 9.5% FOR DESIGN AND LAYOUT, AND 10% MAXIMUM FOR WORK ACCEPTANCE. A PEDESTRIAN CIRCULATION PATH IS ASSUMED TO CROSS THE CURB RAMP WHEN AREA ADJACENT TO THE RAMP IS PAVED AND FREE OF VERTICAL OBSTRUCTIONS THAT WOULD PREVENT PEDESTRIAN PASSAGE. THERE IS NO MAXIMUM FLARE SLOPE FOR A RAMP THAT IS NOT CROSSED BY A PEDESTRIAN CIRCULATION PATH.
- THE BACK SIDE OF A PARALLEL RAMP SHOULD BE GRADED TO A MAXIMUM SLOPE OF 25% TO MATCH EXISTING TERRAIN, UNLESS OTHERWISE SHOWN IN THE CONTRACT DOCUMENTS. WHERE GRADING IS NOT FEASIBLE DUE TO LIMITED ROW OR PHYSICAL CONSTRAINTS, A BACK CURB MAY BE INSTALLED.
- THE DEPARTMENT'S PREFERENCE IS TO INSTALL TWO SEPARATE CURB RAMPS AT A STREET CORNER THAT SERVES TWO SEPARATE PEDESTRIAN CROSSINGS, WITH EACH RAMP ALIGNED TO THE CROSSING THAT IT SERVES. WHERE EXISTING PHYSICAL CONSTRAINTS PREVENT SEPARATE RAMPS, A SINGLE CURB RAMP (I.E., A DIAGONAL CURB RAMP) IS PERMITTED TO SERVE BOTH PEDESTRIAN CROSSINGS.

TURNING SPACE AND CLEAR SPACE NOTES:

 - WHERE A CHANGE IN DIRECTION IS REQUIRED TO UTILIZE A CURB RAMP, A TURNING SPACE SHALL BE PROVIDED AT THE BASE OR THE TOP OF CURB RAMP, AS APPLICABLE. TURNING SPACES SHALL BE PERMITTED TO OVERLAP CLEAR SPACES.
 - WHERE THERE ARE NO VERTICAL CONSTRAINTS AT THE BACK OF SIDEWALK, (E.G., VERTICAL CURBS, BUILDINGS, FENCES) THE TURNING SPACE DIMENSIONS SHALL BE 4'-0" x 4'-0" MINIMUM. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF SIDEWALK, THE TURNING SPACE SHALL BE 4'-0" x 5'-0" MINIMUM. THE 5'-0" DIMENSION SHALL BE IN THE DIRECTION OF THE RAMP RUN.
 - TURNING SPACES SHALL NOT BE DESIGNED WITH A SLOPE GREATER THAN 1.5% IN ANY DIRECTION, WHILE PROVIDING POSITIVE DRAINAGE. THE MAXIMUM SLOPE FOR WORK ACCEPTANCE IS 2.0%.
 - BELOW THE BOTTOM GRADE BREAK OF A CURB RAMP, A CLEAR SPACE OF 4'-0" x 4'-0" MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN CROSSWALK, AND OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE. THE CLEAR SPACE MAY OVERLAP TURNING SPACES, DETECTABLE WARNING SURFACES, AND DROP CURBS.

DETECTABLE WARNING NOTES:

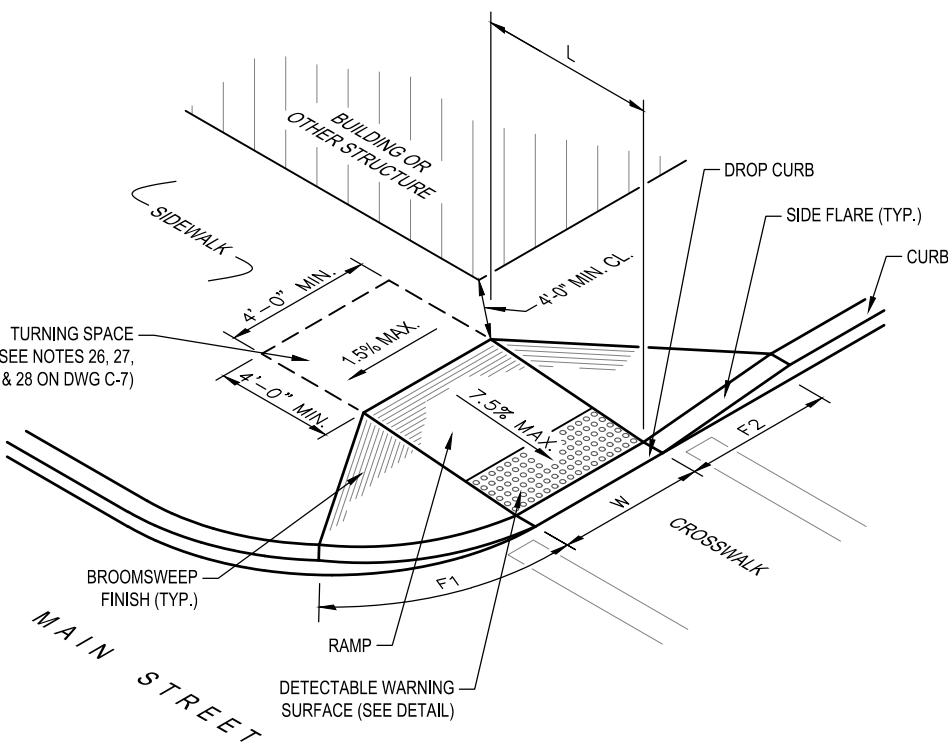
 - DETECTABLE WARNING SURFACES (DWS) SHALL BE PROVIDED AT THE FOLLOWING LOCATIONS ON PEDESTRIAN ACCESS ROUTES:
 - CURB RAMPS AND BLENDED TRANSITIONS AT PEDESTRIAN STREET CROSSINGS.
 - PEDESTRIAN REFUGE ISLANDS (WHERE THE LENGTH OF THE PEDESTRIAN ACCESS ROUTE ACROSS THE REFUGE ISLAND IS GREATER THAN OR EQUAL TO 6').
 - PEDESTRIAN AT-GRADE RAIL CROSSINGS NOT LOCATED WITHIN A STREET OR HIGHWAY.
 - DETECTABLE WARNING SURFACES SHALL BE PROVIDED WHERE THE PEDESTRIAN ACCESS ROUTE CROSSES DRIVEWAYS WITH SIGNAL, YIELD OR STOP CONTROL. DETECTABLE WARNING SURFACES SHALL NOT BE PROVIDED AT CROSSINGS OF UNCONTROLLED DRIVEWAYS.
 - WITH THE EXCEPTION OF THE DETECTABLE WARNING SURFACE TRUNCATED DOME DETAILS ON SHEET C-7, DETECTABLE WARNING DOMES ARE NOT DEPICTED TO SCALE ON THESE SHEETS.
 - DETECTABLE WARNING FIELDS SHALL EXTEND 24" MINIMUM IN THE DIRECTION OF PEDESTRIAN TRAVEL ACROSS THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE, EXCLUDING ANY FLARED SIDES.
 - SOME DETECTABLE WARNING PRODUCTS REQUIRE A CONCRETE BORDER FOR PROPER INSTALLATION. IF REQUIRED, THE BORDER SHALL NOT EXCEED 2". WHERE THE BACK OF THE CURB EDGE IS TOOLED TO PROVIDE A RADIUS, THE BORDER DIMENSION SHALL BE MEASURED FROM THE INSIDE EDGE OF THE CURB RADIUS. BORDERS CANNOT BE INCLUDED AS PART OF THE 24" MINIMUM DIMENSION DESCRIBED IN NOTE 33.
 - WHERE CURB IS NOT USED, THE EDGE OF PAVEMENT SHALL BE SUBSTITUTED FOR THE BACK OF CURB FOR PLACEMENT OF DETECTABLE WARNINGS.
 - ON SLOPES OF 5% OR GREATER, THE ROWS OF DOMES SHALL BE ALIGNED TO BE PERPENDICULAR OR RADIAL TO THE LOWER GRADE BREAK ON THE RAMP RUN. WHERE DOMES ARE ARRAYED RADIALLY, THEY MAY DIFFER IN DIAMETER AND CENTER-TO-CENTER SPACING WITHIN THE RANGES SPECIFIED ON SHEET C-7. DOME ALIGNMENT THAT IS PERPENDICULAR OR RADIAL TO THE LOWER GRADE BREAK IS NOT REQUIRED ON SLOPES OF LESS THAN 5%.
 - THE DETECTABLE WARNING FIELD SHALL BE THE COLOR SPECIFIED IN THE CONTRACT DOCUMENTS OR MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS. DETECTABLE WARNING SURFACES SHALL CONTRAST VISUALLY WITH ADJACENT GUTTER, STREET OR HIGHWAY, OR PEDESTRIAN ACCESS ROUTE SURFACE, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT.



CURB RAMP TRANSITIONS

CURB RAMP SCHEDULE									
CURB RAMP #	W	L	F1	F2	NON-COMPLIANT ELEMENT(S)				
					RAMP CROSS SLOPE (2.0% MAX.)	RAMP RUNNING SLOPE (8.3% MAX.)	SIDE FLARE SLOPE (F1) (9.5% MAX. SEE NOTE 1)	SIDE FLARE SLOPE (F2) (9.5% MAX. SEE NOTE 1)	COUNTER SLOPE (5.0% MAX.)
1	4'-0"	3'-0"	2'-3"	5'-8"	7.5%	10.7%	-	15.5%	-
4	4'-0"	3'-0"	2'-2"	2'-1"	7.8%	8.7%	-	31.1%	3.2%
5	4'-11" (EXIST. CURB TO REMAIN)	2'-0"	3'-10" (EXIST. CURB TO REMAIN)	5'-6" (EXIST. CURB TO REMAIN)	12.3%	-	-	21.6%	10.7%
6	5'-4" (EXIST. CURB TO REMAIN)	4'-11"	3'-4"	5'-4" (EXIST. CURB TO REMAIN)	6.4%	16.6%	13.7%	17.6%	8.2%
7	4'-0" (EXIST. CURB TO REMAIN)	5'-4"	2'-0" (EXIST. CURB TO REMAIN)	2'-2"	5.5%	10.0%	-	-	-

- NOTES:
- SIDE FLARE LENGTH AS REQUIRED TO ACHIEVE 9.5% MAXIMUM SLOPE OR 15'-1" MAXIMUM. THERE IS NO MAXIMUM SIDE FLARE SLOPE REQUIREMENT WHERE THERE ARE VERTICAL OBSTRUCTIONS ADJACENT TO THE RAMP THAT PREVENT PEDESTIAN PASSAGE.
 - REFER TO DWG. C-7 FOR ADDITIONAL DETAILS AND NOTES.

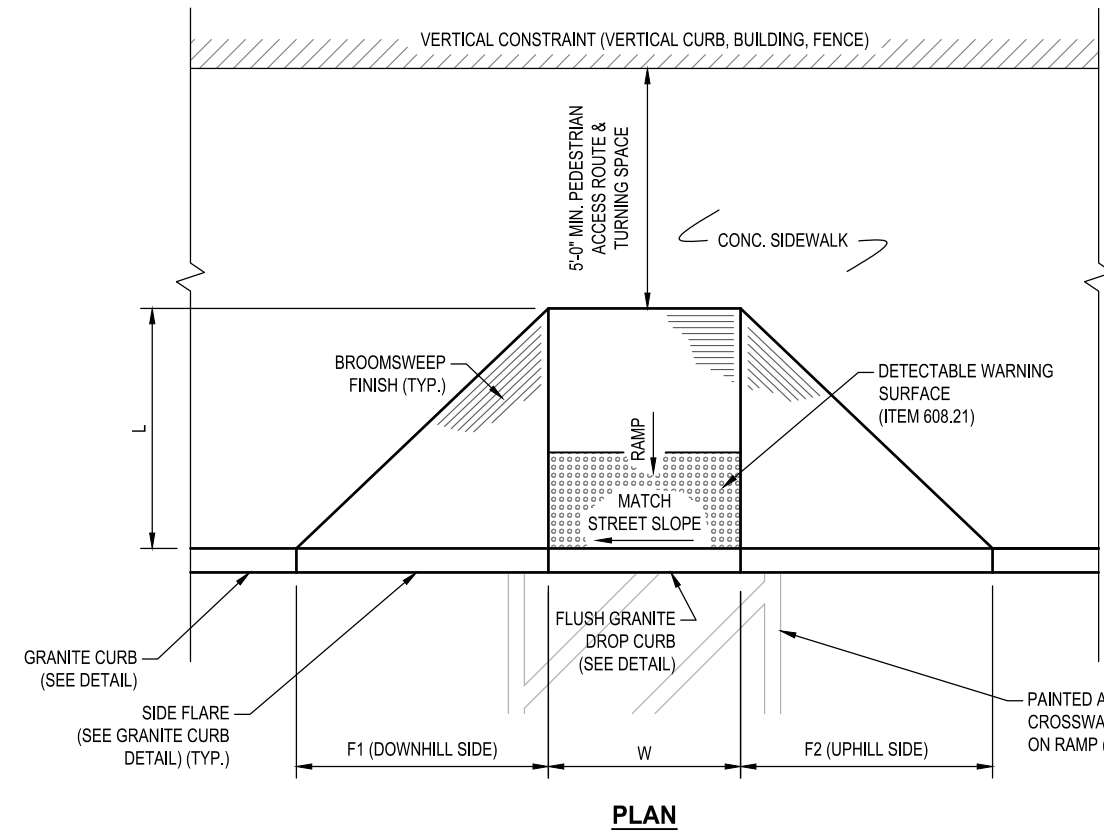


CURB RAMP #1, 4, 5, 6, & 7

N.T.S.

CURB RAMP SCHEDULE									
CURB RAMP #	W	L	F1	F2	NON-COMPLIANT ELEMENT(S)				
					RAMP CROSS SLOPE (2.0% MAX.)	RAMP RUNNING SLOPE (8.3% MAX.)	SIDE FLARE SLOPE (F1) (9.5% MAX. SEE NOTE 1)	SIDE FLARE SLOPE (F2) (9.5% MAX. SEE NOTE 1)	COUNTER SLOPE (5.0% MAX.)
2	4'-0"	4'-6"	SEE NOTE 1	SEE NOTE 1	2.25%	19.2%	-	-	25.7%
6A	5'-0"	5'-0"	3'-3"	2'-7"	-	16.4%	10.9%	-	6.4%
8	4'-0"	5'-0"	SEE NOTE 1	SEE NOTE 1	4.9%	11.3%	-	-	5.1%
8	5'-0"	5'-4"	SEE NOTE 1	SEE NOTE 1	-	18.4%	-	-	10.4%
10	5'-0"	5'-11"	4'-2" @ 3.9%	SEE NOTE 1	-	16.2%	-	-	21.2%

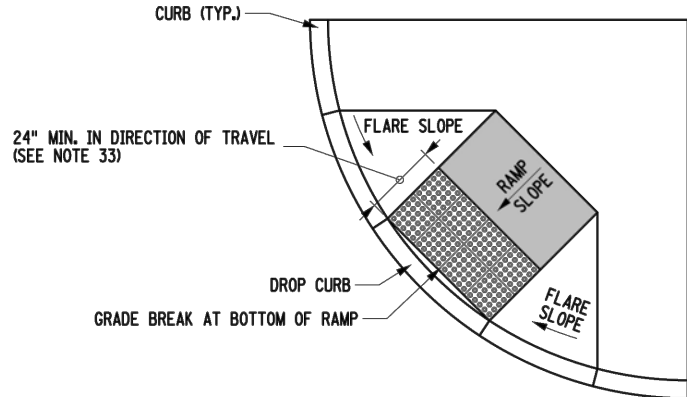
- *TRANSITION TO TOP OF LOAN'S LANE SIDE FLARE.
- NOTES:
- SIDE FLARE LENGTH AS REQUIRED TO ACHIEVE 9.5% MAXIMUM SLOPE OR 15'-1" MAXIMUM. THERE IS NO MAXIMUM SIDE FLARE SLOPE REQUIREMENT WHERE THERE ARE VERTICAL OBSTRUCTIONS ADJACENT TO THE RAMP THAT PREVENT PEDESTIAN PASSAGE.
 - REFER TO DWG. C-7 FOR ADDITIONAL DETAILS AND NOTES.



CURB RAMP #2, 6A, 8, 9, & 10

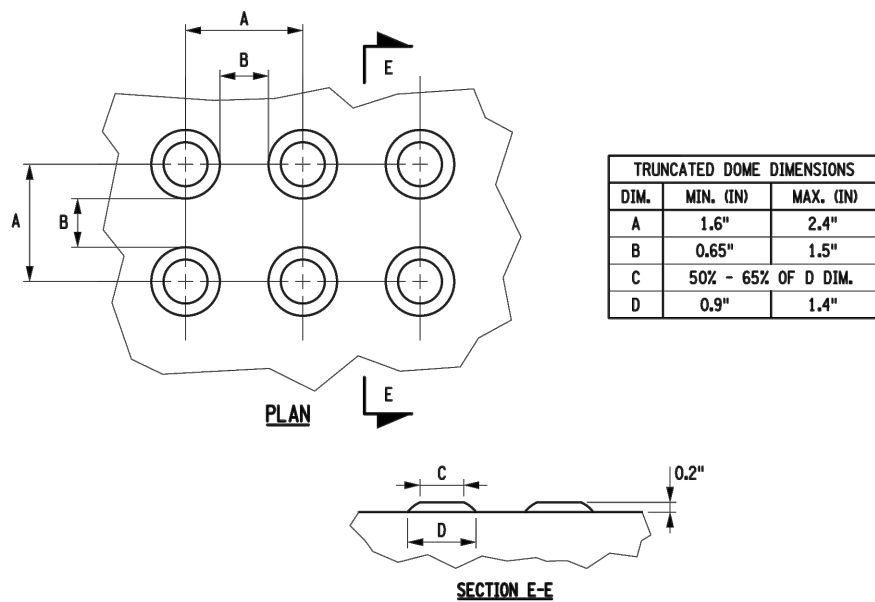
SCALE: 1/4" = 1'-0"

NOTE: DETECTABLE WARNING SURFACES SHALL BE EMBEDDED PLASTIC SHEETS (BRICK RED).



DETECTABLE WARNING SURFACE PLACEMENT - OPTION 1

DETECTABLE WARNING SURFACE (DWS) PLACEMENT



NOTE: DETAILS (EXCEPT FOR DETAILS 1, 2, & 3) AND NOTES HEREON REFERENCE N.Y.S.D.O.T. STANDARD SHEETS 608-01.

TITLE			CURB RAMP DETAILS & NOTES	
PROJECT			PROPOSED MAIN STREET SIDEWALKS IMPROVEMENT PROJECT	
			FEDERAL AID PROJECT NYSDOT P.I.N. 8761.52	
			VILLAGE OF DOBBS FERRY, WESTCHESTER COUNTY, NEW YORK	
REV.	DATE	DESCRIPTION	SCALE	DATE
8	1/24/22	PER NYSDOT	AS NOTED	6/8/19
7	1/14/22	PER NYSDOT		
6	12/14/21	CROSSWALKS ADDED		
5	11/12/21	PER VILLAGE		
4	2/12/20	ISSUED FOR BID		
3	1/29/20	PER NYSDOT		
2	9/8/19	PER NYSDOT		
1	8/14/19	NYSDOT SUBMISSION		
UNAUTHORIZED ALTERATIONS OR ADDITIONS TO THIS DRAWING IS A VIOLATION OF SECTION 7209 (2) OF THE NEW YORK STATE EDUCATION LAW. THIS PLAN IS NULL AND VOID FOR CONSTRUCTION PURPOSES WITHOUT THE SIGNATURE AND SEAL OF THE DESIGN ENGINEER.			Putnam Business Park 1689 Route 22 Brewster, New York 10809 Tel: (845) 279-2220	
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