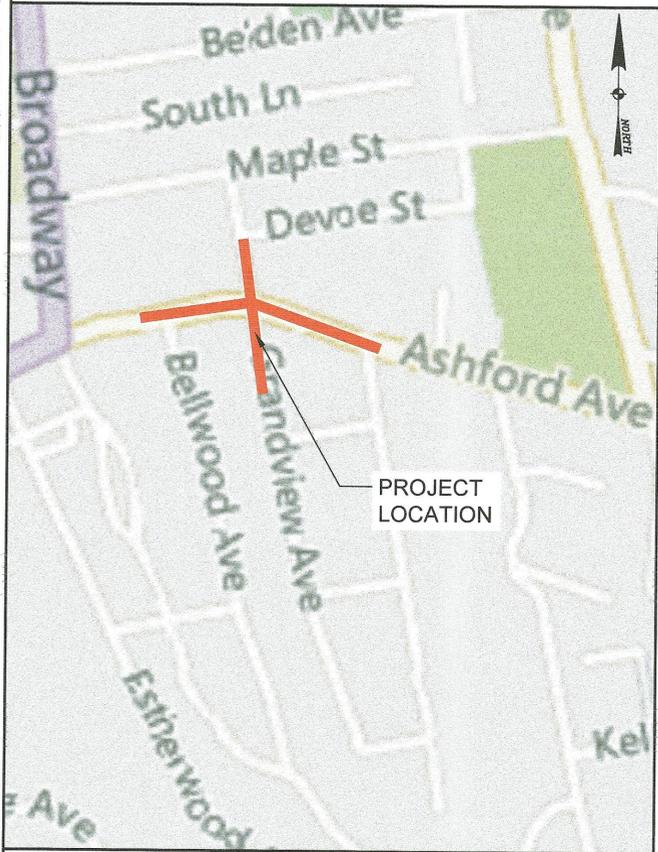


# VILLAGE OF DOBBS FERRY WESTCHESTER COUNTY, NEW YORK

## PLANS FOR PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION OF ASHFORD AVE AND GRANDVIEW AVE/STORM ST



LOCATION MAP  
N.T.S.

SEPTEMBER 2023

N+ P NO. 23017

IT IS A VIOLATION OF NEW YORK STATE EDUCATION LAW ARTICLE 145, PROFESSIONAL ENGINEERING AND LAND SURVEYING, SECTION 7209 FOR ANY PERSON, UNLESS HE IS ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE SEAL OF AN ENGINEER OR LAND SURVEYOR IS ALTERED, THE ALTERING ENGINEER OR LAND SURVEYOR SHALL AFFIX TO THE ITEM HIS SEAL AND THE NOTATION "ALTERED BY" FOLLOWED BY HIS SIGNATURE AND THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

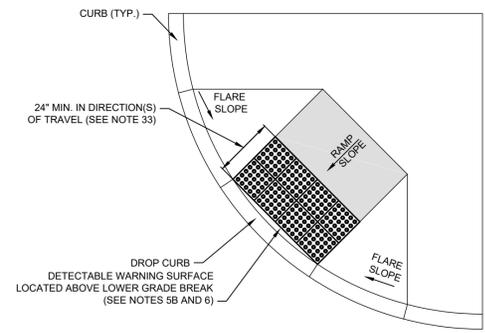


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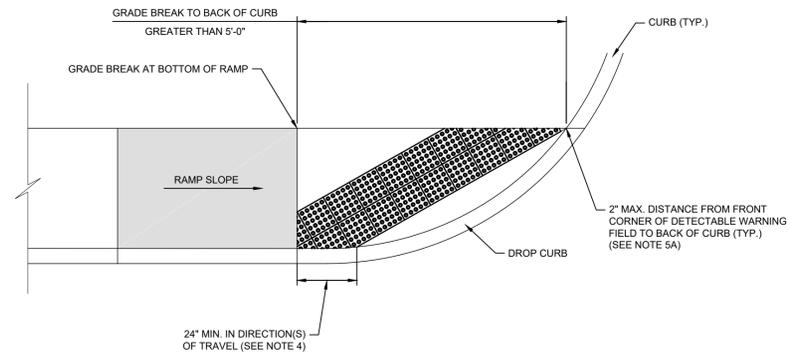
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PDT:230913-12:33 | DIR:C:\PROJECTS\23017\DWG\ROADWAY\23017\_COVDWG | DST:ARCH. HP 17200 PLOTTER | PSE:MAP\_2009\_MTA-LARGE-SCALE\_TPO - COLOR.CTB | PSCALE 1:1 | BY:LW

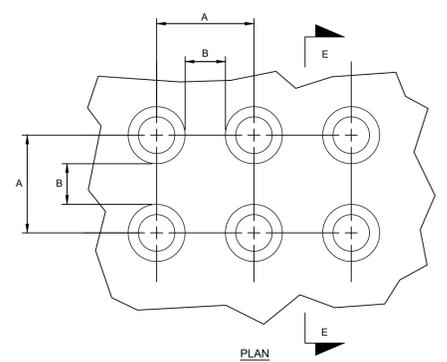
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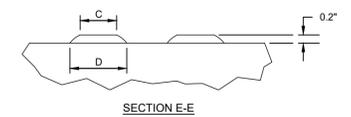
**DETECTABLE WARNING SURFACE PLACEMENT - OPTION 1**



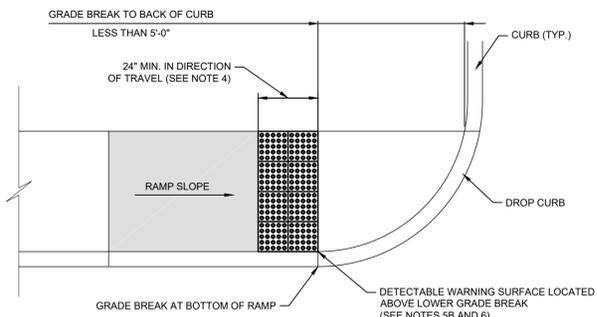
**DETECTABLE WARNING SURFACE PLACEMENT - OPTION 4**



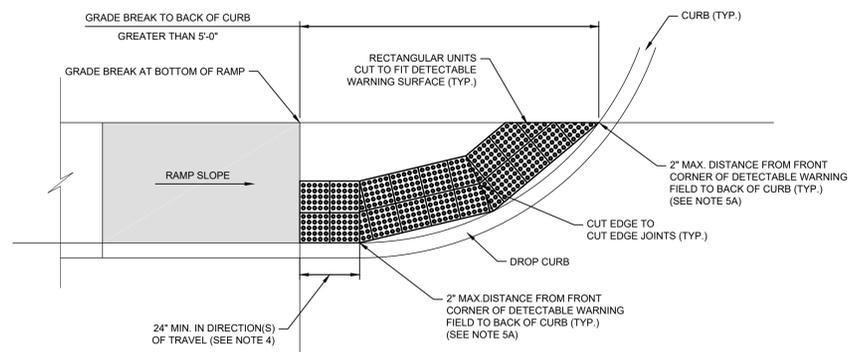
TRUNCATED DOME DIMENSIONS		
DIM.	MIN. (IN)	MAX. (IN)
A	1.6"	2.4"
B	0.65"	1.5"
C	50% - 65% OF D DIM.	
D	0.9"	1.4"



**DETECTABLE WARNING SURFACE (DWS) TRUNCATED DOME DETAILS**



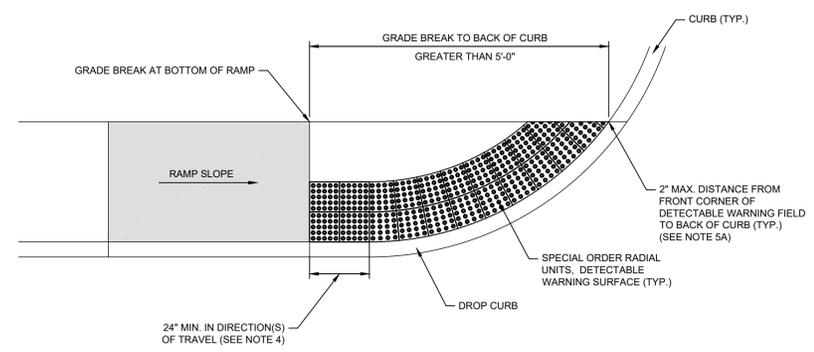
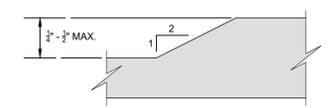
**DETECTABLE WARNING SURFACE PLACEMENT - OPTION 2**



**DETECTABLE WARNING SURFACE PLACEMENT - OPTION 5**



**VERTICAL SURFACE DISCONTINUITIES**  
SEE NOTE 5 ON SHEET 1 OF 12



**DETECTABLE WARNING SURFACE PLACEMENT - OPTION 3**

**DETECTABLE WARNING NOTES:**

- DETECTABLE WARNING SURFACES (DWS) SHALL BE PROVIDED AT THE FOLLOWING LOCATIONS OF PEDESTRIAN ACCESS ROUTES:
  - CURB RAMPS ARE BLENDED TRANSITIONS AT PEDESTRIAN STREET CROSSINGS.
  - PEDESTRIAN REFUGE ISLANDS (WHERE THE LENGTH OF THE PEDESTRIAN ACCESS ROUTE ACROSS THE REFUGE ISLAND IS GREATER THAN OR EQUAL TO 6 FEET).
  - PEDESTRIAN AT-GRADE RAIL CROSSINGS NOT LOCATED WITHIN A STREET OR HIGHWAY.
- DETECTABLE WARNING SURFACES SHALL BE PROVIDED WHERE THE PEDESTRIAN ACCESS ROUTE CROSSES DRIVEWAYS WITH SIGNAL, YIELD OR STOP CONTROL. DETECTABLE WARNING SURFACES SHALL NOT BE PROVIDED AT CROSSINGS OF UNCONTROLLED DRIVEWAYS.
- WITH THE EXCEPTION OF THE "DETECTABLE WARNING SURFACE TRUNCATED DOME DETAILS" DETECTABLE WARNING DOMES ON THIS SHEET ARE NOT DEPICTED TO SCALE.
- DETECTABLE WARNING FIELDS SHALL EXTEND 24" MINIMUM IN THE DIRECTION OF PEDESTRIAN TRAVEL ACROSS THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE, EXCLUDING AND FLARED SIDES.
- SOME DETECTABLE WARNING PRODUCTS REQUIRE A CONCRETE BORDER FOR PROPER INSTALLATION. BORDERS CANNOT BE INCLUDED AS PART OF THE 24" MINIMUM DIMENSION DESCRIBED IN NOTE 4.
  - WHEN PLACED AT THE BACK OF CURB, DETECTABLE WARNING FIELDS SHOULD BE PLACED AS CLOSE TO THE BACK OF CURB AS PRACTICABLE. BOTH FRONT CORNERS OF THE DETECTABLE WARNING FIELD SHALL NOT BE LOCATED MORE THAN 2" FROM THE BACK OF CURB. WHERE THE BACK OF THE CURB EDGE IS TOoled TO PROVIDE A RADIUS, THE BORDER DIMENSION SHALL BE MEASURED FROM THE INSIDE EDGE OF THE CURB RADIUS. WHERE CURB IS NOT USED, THE EDGE OF PAVEMENT SHALL BE SUBSTITUTED FOR THE BACK OF CURB FOR PLACEMENT OF DETECTABLE WARNINGS.
  - WHEN PLACED ABOVE THE LOWER GRADE BREAK OF A CURB RAMP, DETECTABLE WARNING UNITS SHOULD BE PLACED AS CLOSE TO THE JOINT AS PRACTICABLE. BOTH FRONT CORNERS OF RECTILINEAR DETECTABLE WARNING FIELDS SHALL NOT BE LOCATED MORE THAN 2" FROM THE JOINT.
  - WHEN RADIAL DWS UNITS ARE PLACED AT THE BACK OF CURB, THE FRONT EDGE OF THE DWS FIELD SHOULD BE AS CLOSE AS POSSIBLE TO THE BACK OF CURB, I.E., THE RADIUS OF THE FRONT OF THE DWS FIELD SHOULD MATCH THE RADIUS ALONG THE BACK OF CURB AS CLOSELY AS POSSIBLE. THE OUTSIDE CORNERS OF THE DWS FIELD MUST BE LOCATED NO MORE THAN 2 INCHES FROM THE BACK OF CURB.
- ON SLOPES OF 5% OR GREATER, THE ROWS OF DOMES SHALL BE ALIGNED TO BE PERPENDICULAR OR RADIAL TO THE LOWER GRADE BREAK ON THE RAMP RUN. WHERE DOMES ARE ARRAYED RADIALLY, THEY MAY DIFFER IN DIAMETER AND CENTER-TO-CENTER SPACING WITHIN THE RANGES SPECIFIED ON THIS SHEET. DOME ALIGNMENT THAT IS PERPENDICULAR OR RADIAL TO THE LOWER GRADE BREAK IS NOT REQUIRED ON SLOPES OF LESS THAN 5%.
- THE DETECTABLE WARNING FIELD SHALL BE THE COLOR SPECIFIED IN THE CONTRACT DOCUMENTS OR MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS. DETECTABLE WARNING SURFACES CONTRAST VISUALLY WITH ADJACENT GUTTER, STREET OR HIGHWAY, OR PEDESTRIAN ACCESS ROUTE SURFACE, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT.

No.	DATE	REVISION	BY:

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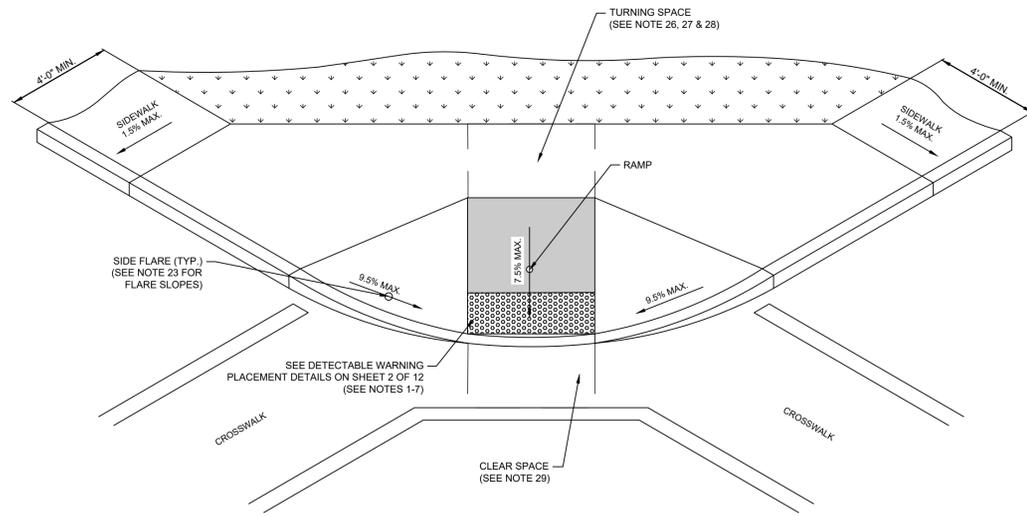
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CLIENT: **VILLAGE OF DOBBS FERRY**  
 112 MAIN STREET  
 DOBBS FERRY, NEW YORK 10522

DWN. BY: LW  
 CHKD BY: JD  
 DATE: SEP 2023  
 JOB No.: 23017  
 CADD: 23017\_DET.DWG  
 SCALE: AS SHOWN

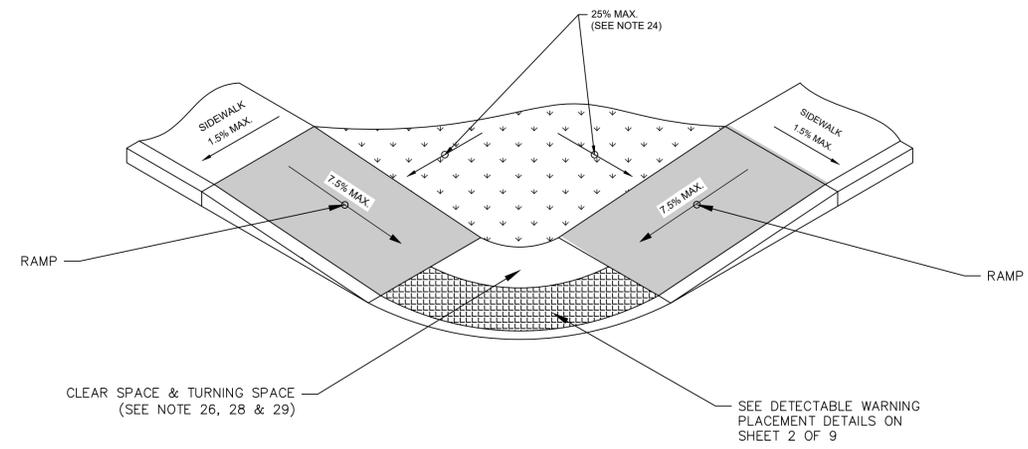
DRAWING TITLE: **MISCELLANEOUS DETAILS - 1**  
 FOR  
 PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION  
 OF ASHFORD AVE AND GRANDVIEW AVE / STORM ST  
 VILLIAGE OF DOBBS FERRY, NY 10522

DRAWING NUMBER: **MD - 1**  
 SHEET: 2 OF 11



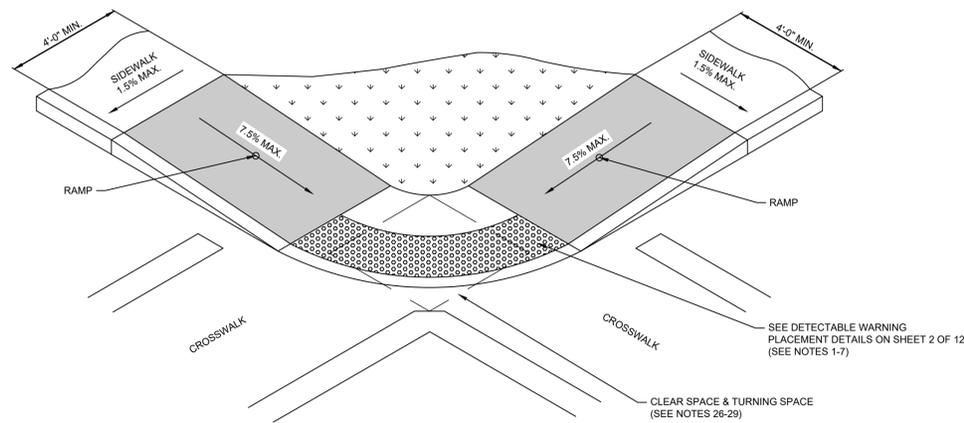
**CURB RAMP CONFIGURATION: TYPE 8**

NOT TO SCALE



**CURB RAMP CONFIGURATION: TYPE 9B**

NOT TO SCALE



**CURB RAMP CONFIGURATION: TYPE 4**

NOT TO SCALE

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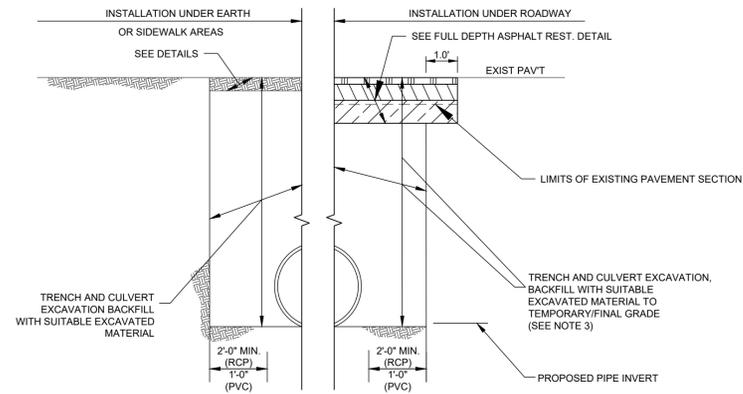
CLIENT:

VILLAGE OF DOBBS FERRY  
 112 MAIN STREET  
 DOBBS FERRY, NEW YORK 10522

DWN. BY: LW  
 CHKD BY: JD  
 DATE: SEP 2023  
 JOB No.: 23017  
 CADD: 23017\_DET.DWG  
 SCALE: AS SHOWN

DRAWING TITLE:  
**MISCELLANEOUS DETAILS - 2**  
 FOR  
 PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION  
 OF ASHFORD AVE AND GRANDVIEW AVE / STORM ST  
 VILLIAGE OF DOBBS FERRY, NY 10522

DRAWING NUMBER:  
**MD - 2**  
 SHEET: 3 OF 11



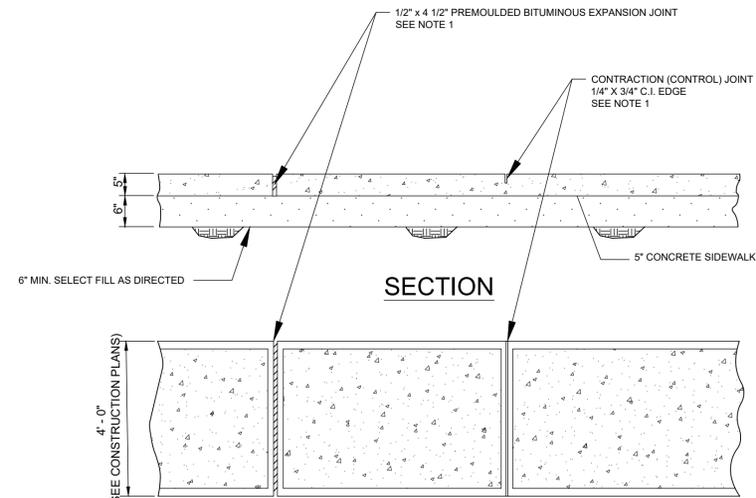
**DRAINAGE PIPE INSTALLATION**

**ITEM 9**

NOT TO SCALE

**DRAINAGE TRENCHING NOTES:**

- ALL REINFORCED CONCRETE PIPE TO BE INSTALLED UNDER THIS CONTRACT SHALL HAVE A MINIMUM TRENCH WIDTH OF 4'-0" + PIPE DIAMETER FOR PROPER INSTALLATION, BACKFILL AND COMPACTION PROCEDURES TO TAKE PLACE.
- ALL POLYVINYL CHLORIDE (PVC) PIPE INSTALLATIONS SHALL ONLY TAKE PLACE WITHIN THE SIDEWALK AREA, AS DEPICTED ON THE CONSTRUCTION PLANS HAVING A TRENCH WIDTH OF 2'-0" + PIPE DIAMETER.
- TEMPORARY ASPHALT MAY BE PLACED BETWEEN THE BACKFILL AND TEMPORARY/FINAL GRADE, ONLY IF DETERMINED NECESSARY BY THE ENGINEER IN CHARGE; MAX 3 INCHES, TO BE PAID FOR UNDER 36T



**SECTION**

**PLAN**

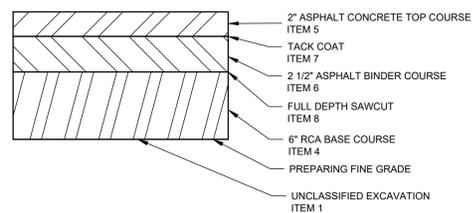
**CONCRETE SIDEWALK**

**ITEM 14**

NOT TO SCALE

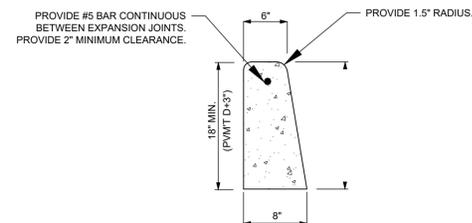
**SIDEWALK NOTES:**

- CONTROL JOINTS ARE TO BE PLACED EVERY 5'-0" AND EXPANSION JOINTS ARE TO BE PLACED EVERY 20', OR AS DIRECTED BY THE ENGINEER.
- ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED 5" THICK.
- ALL SIDEWALKS IN INTERSECTIONS AND IN SMALL RADII SHALL BE PLACED MONOLITHICALLY AND SHALL BE 5 INCHES THICK UNREINFORCED CONCRETE FROM THE PC TO THE PT. THE AREA TO BE PAID FOR THE SIDEWALK ITEM SHALL NOT INCLUDE THE CURB AND GUTTER.



**FULL DEPTH ASPHALT RESTORATION DETAIL**

NOT TO SCALE



**CONCRETE CURB DETAIL**

**ITEM 16**

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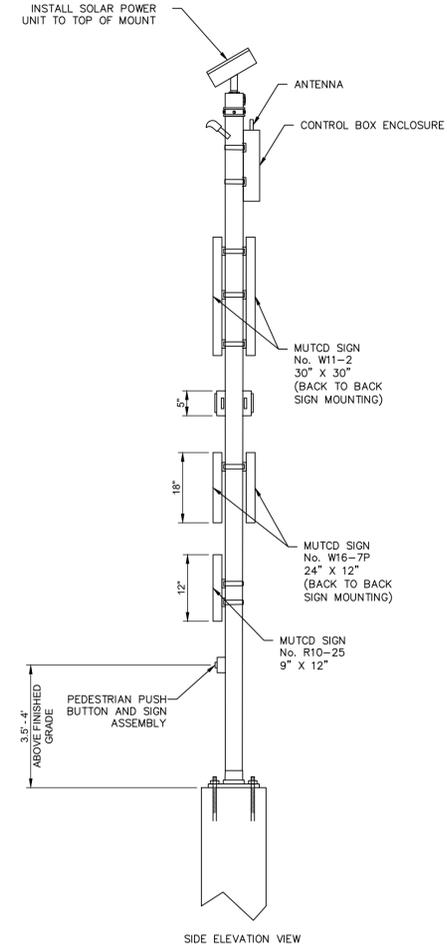
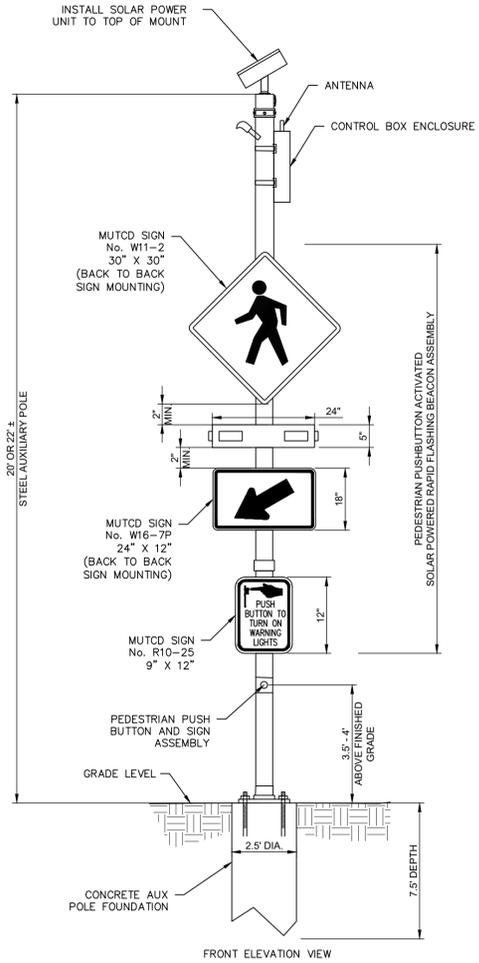
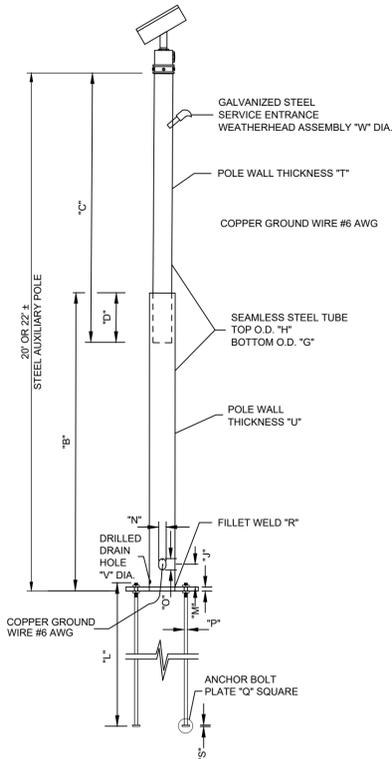
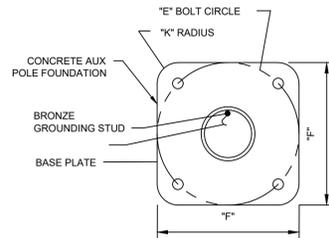
CLIENT: **VILLAGE OF DOBBS FERRY**  
 112 MAIN STREET  
 DOBBS FERRY, NEW YORK 10522

DWN. BY: LW  
 CHKD BY: JD  
 DATE: SEP 2023  
 JOB No.: 23017  
 CADD: 23017\_DET.DWG  
 SCALE: AS SHOWN

DRAWING TITLE: **MISCELLANEOUS DETAILS - 3**  
 FOR  
 PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION  
 OF ASHFORD AVE AND GRANDVIEW AVE / STORM ST  
 VILLIAGE OF DOBBS FERRY, NY 10522

DRAWING NUMBER:  
**MD - 3**  
 SHEET: 4 OF 11

ITEM	DIM	ENGLISH
POLE HEIGHT	"A"	20'
LOWER SECTION	"B"	11.5'
UPPER SECTION	"C"	10.5'
OVERLAP	"D"	2.0'
BOLT CIRCLE	"E"	12.75"
BASE PLATE	"F"	12.75"
LOWER SECT. DIA.	"G"	4.5"
UPPER SECT. DIA.	"H"	4.0"
BASE PLATE THICK	"J"	1"
ROUNDING	"K"	2"
ANCH BOLT LENGTH	"L"	48"
HANDHOLE CLEAR.	"M"	18"
HANDHOLE WIDTH	"N"	3"
HANDHOLE HEIGHT	"O"	5"
BOLT DIAMETER	"P"	.75"
PLATE DIM.	"Q"	3"
WELD	"R"	.237"
PLATE THICK	"S"	1"
WALL THICKNESS	"T/U"	.226"/.237"
DRAIN HOLE DIA.	"V"	.375"
WEATHERHEAD DIA.	"W"	1.5"



**SOLAR POWERED RAPID FLASHING BEACON**

**ITEM 23**

NOT TO SCALE

**NOTES:**

- ITEM 23 - SOLAR POWERED RAPID FLASHING BEACON SHALL INCLUDE ALL EQUIPMENT AND MATERIALS NECESSARY TO COMPLETE THIS INSTALLATION INCLUDING BUT NOT LIMITED TO: RECTANGULAR RAPID FLASHING BEACON, SOLAR POWER EQUIPMENT AND BATTERIES, DECORATIVE POLE, POLE FOUNDATION, PEDESTRIAN SIGNS AND ASSEMBLY, PEDESTRIAN PUSH BUTTON ASSEMBLY, WIRING, HARDWARE, ACCESSORIES, ETC.

No.	DATE	REVISION	BY:

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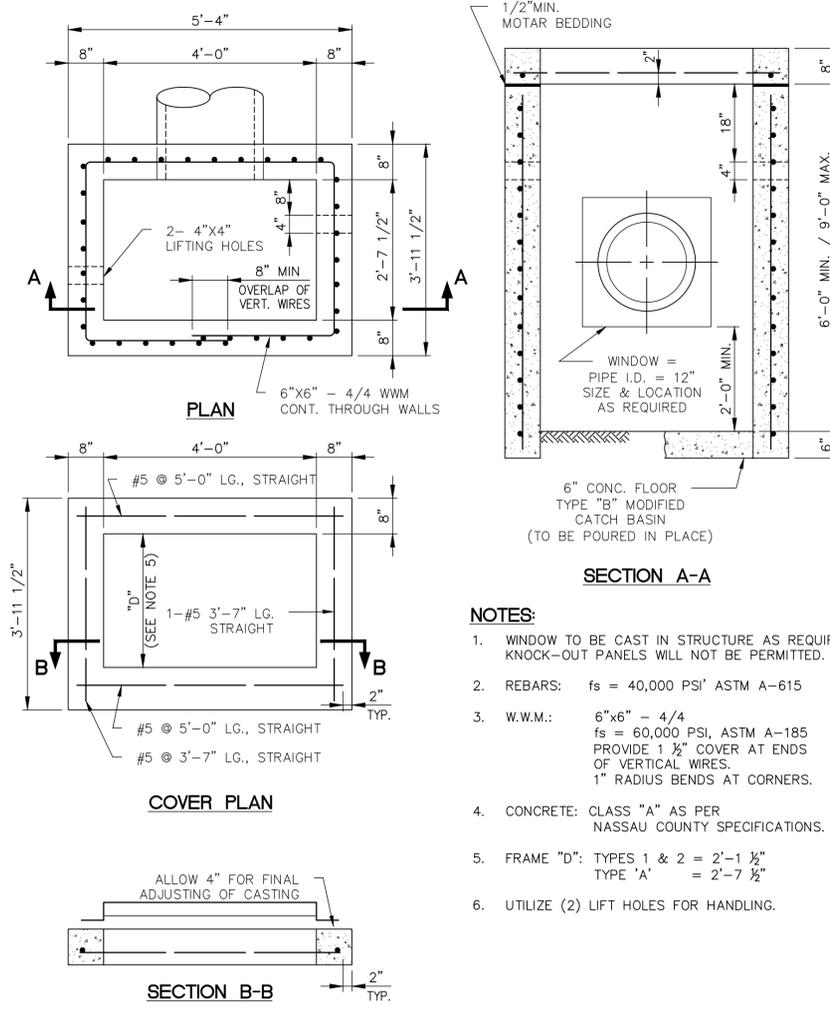
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DWN BY: LW  
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DATE: SEP 2023  
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SCALE: AS SHOWN

DRAWING TITLE: MISCELLANEOUS DETAILS - 4  
FOR  
PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION OF ASHFORD AVE AND GRANDVIEW AVE / STORM ST VILLIAGE OF DOBBS FERRY, NY 10522

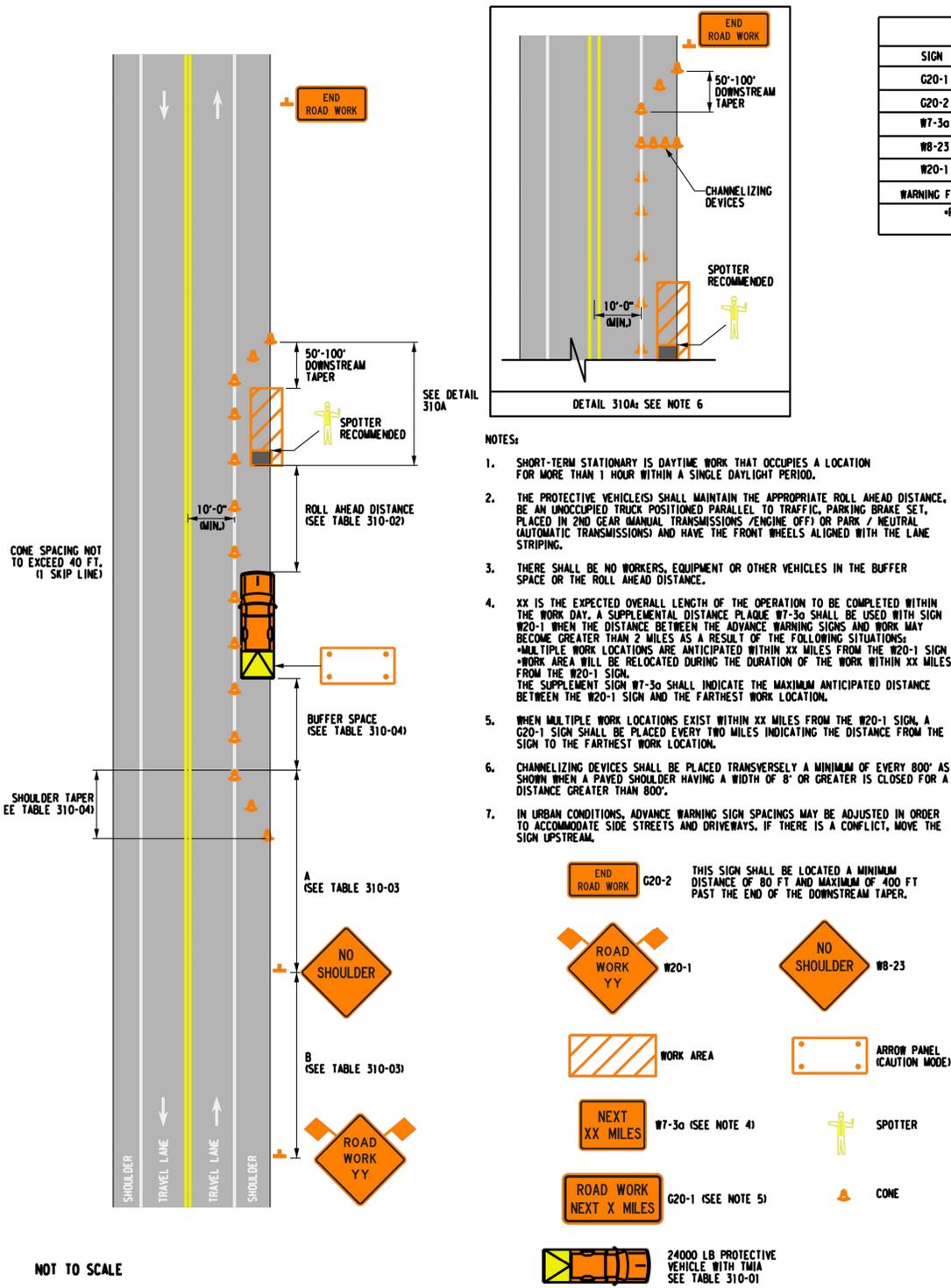
DRAWING NUMBER: MD - 4  
SHEET: 5 OF 11



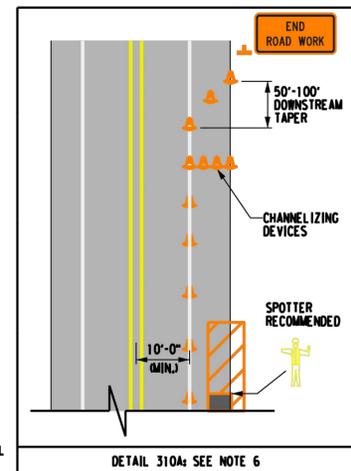
- NOTES:**
1. WINDOW TO BE CAST IN STRUCTURE AS REQUIRED. KNOCK-OUT PANELS WILL NOT BE PERMITTED.
  2. REBARS:  $f_s = 40,000$  PSI' ASTM A-615
  3. W.W.M.: 6"x6" - 4/4  
 $f_s = 60,000$  PSI, ASTM A-185  
 PROVIDE 1 1/2" COVER AT ENDS OF VERTICAL WIRES.  
 1" RADIUS BENDS AT CORNERS.
  4. CONCRETE: CLASS "A" AS PER NASSAU COUNTY SPECIFICATIONS.
  5. FRAME "D": TYPES 1 & 2 = 2'-1 1/2"  
 TYPE 'A' = 2'-7 1/2"
  6. UTILIZE (2) LIFT HOLES FOR HANDLING.

**PRECAST REINFORCED CONCRETE  
 CATCH BASINS TYPE 'A' AND 'A' MODIFIED**  
 ITEM 13  
 NOT TO SCALE

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No.	DATE	REVISION	BY:							SHEET: 6 OF 11			



NOT TO SCALE



- NOTES:**
- SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
  - THE PROTECTIVE VEHICLE(S) SHALL MAINTAIN THE APPROPRIATE ROLL AHEAD DISTANCE. BE AN UNOCCUPIED TRUCK POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS / ENGINE OFF) OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS) AND HAVE THE FRONT WHEELS ALIGNED WITH THE LANE STRIPING.
  - THERE SHALL BE NO WORKERS, EQUIPMENT OR OTHER VEHICLES IN THE BUFFER SPACE OR THE ROLL AHEAD DISTANCE.
  - XX IS THE EXPECTED OVERALL LENGTH OF THE OPERATION TO BE COMPLETED WITHIN THE WORK DAY. A SUPPLEMENTAL DISTANCE PLaque W7-3a SHALL BE USED WITH SIGN W20-1 WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK MAY BECOME GREATER THAN 2 MILES AS A RESULT OF THE FOLLOWING SITUATIONS:  
 \*MULTIPLE WORK LOCATIONS ARE ANTICIPATED WITHIN XX MILES FROM THE W20-1 SIGN  
 \*WORK AREA WILL BE RELOCATED DURING THE DURATION OF THE WORK WITHIN XX MILES FROM THE W20-1 SIGN.  
 THE SUPPLEMENT SIGN W7-3a SHALL INDICATE THE MAXIMUM ANTICIPATED DISTANCE BETWEEN THE W20-1 SIGN AND THE FARTHEST WORK LOCATION.
  - WHEN MULTIPLE WORK LOCATIONS EXIST WITHIN XX MILES FROM THE W20-1 SIGN, A G20-1 SIGN SHALL BE PLACED EVERY TWO MILES INDICATING THE DISTANCE FROM THE SIGN TO THE FARTHEST WORK LOCATION.
  - CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.
  - IN URBAN CONDITIONS, ADVANCE WARNING SIGN SPACINGS MAY BE ADJUSTED IN ORDER TO ACCOMMODATE SIDE STREETS AND DRIVEWAYS. IF THERE IS A CONFLICT, MOVE THE SIGN UPSTREAM.

**TABLE 310-05: REQUIRED SIGN SIZES\***

SIGN	NON-FREEWAY	FREEWAY
G20-1	36x18	48x24
G20-2	36x18	48x24
W7-3a	24x18	36x30
WB-23	36x36	48x48
W20-1	36x36	48x48
WARNING FLAG	18x18	18x18

\*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.

**TABLE 310-01: PROTECTIVE VEHICLE REQUIREMENTS**

CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEWAY		
		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
LANE CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	P
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	P	SEE NOTE 2
SHOULDER CLOSURE OR ENCROACHMENT	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P	P
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	P	SEE NOTE 2

**LEGEND**  
 P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER. IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY.  
 TMIA: TMIA REQUIRED

**NOTES:**  
 1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT  
 2. EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED

**TABLE 310-02: ROLL AHEAD DISTANCE**

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	MIN	MAX
≥ 55	120/3	200/5
45 - 50	80/2	160/4
≤ 40	40/1	120/3

**TABLE 310-03: ADVANCE WARNING SIGN SPACING**

ROAD TYPE	DISTANCE BETWEEN SIGNS		SIGN LEGEND	
	A (FT.)	B (FT.)	XX	YY
URBAN (≤ 30 MPH*)	100	100	AHEAD	AHEAD
URBAN (35-40 MPH*)	200	200	AHEAD	AHEAD
URBAN (≥ 45 MPH*)	350	350	1000 FT.	AHEAD
RURAL	500	500	1500 FT.	1000 FT.

\* PRECONSTRUCTION POSTED SPEED LIMIT

**TABLE 310-04: LONGITUDINAL BUFFER SPACE AND TAPER LENGTHS**

PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE DISTANCE (FT.) / # OF SKIP LINES	TAPER LENGTH: L (FT.) / # OF SKIP LINES / # OF CHANNELIZING DEVICES			SHOULDER TAPER LENGTH: L/3 (FT.) / # OF SKIP LINES / # OF CHANNELIZING DEVICES		
		FOR LANE WIDTH IN FT. (LATERAL SHIFT OF TRAFFIC FLOW PATH)			FOR SHOULDER WIDTH		
		10	11	12	≤ 4 FT.	5 - 7 FT.	≥ 8 FT.
25	155/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2	40/1/2
30	200/5	160/4/5	160/4/5	200/5/6	40/1/2	40/1/2	40/1/2
35	250/6	200/5/6	240/6/7	240/6/7	40/1/2	40/1/2	80/2/3
40	305/8	280/7/8	320/8/9	320/8/9	40/1/2	80/2/3	80/2/3
45	360/9	440/11/12	520/13/14	560/14/15	80/2/3	80/2/3	120/3/4
50	425/11	520/13/14	560/14/15	600/15/16	80/2/3	120/3/4	160/4/5
55	495/13	560/14/15	600/15/16	680/17/18	80/2/3	120/3/4	160/4/5

**NOTE:**  
 1. THE CONTRACTOR SHALL SUBMIT SITE SPECIFIC WORK ZONE TRAFFIC CONTROL SCHEMES TO VILLAGE FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION. A FULL CLOSURE WILL NOT BE ACCEPTABLE.

NYS DOT STANDARD SHEET  
 REF NO. 619-310

No.	DATE	REVISION	BY:
-	-	-	-

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CLIENT:

VILLAGE OF DOBBS FERRY  
 112 MAIN STREET  
 DOBBS FERRY, NEW YORK 10522

DWN BY:	LW
CHKD BY:	JD
DATE:	SEP 2023
JOB No.:	23017
CADD:	23017_GP_4BUMPPOINTS
SCALE:	AS SHOWN

DRAWING TITLE:

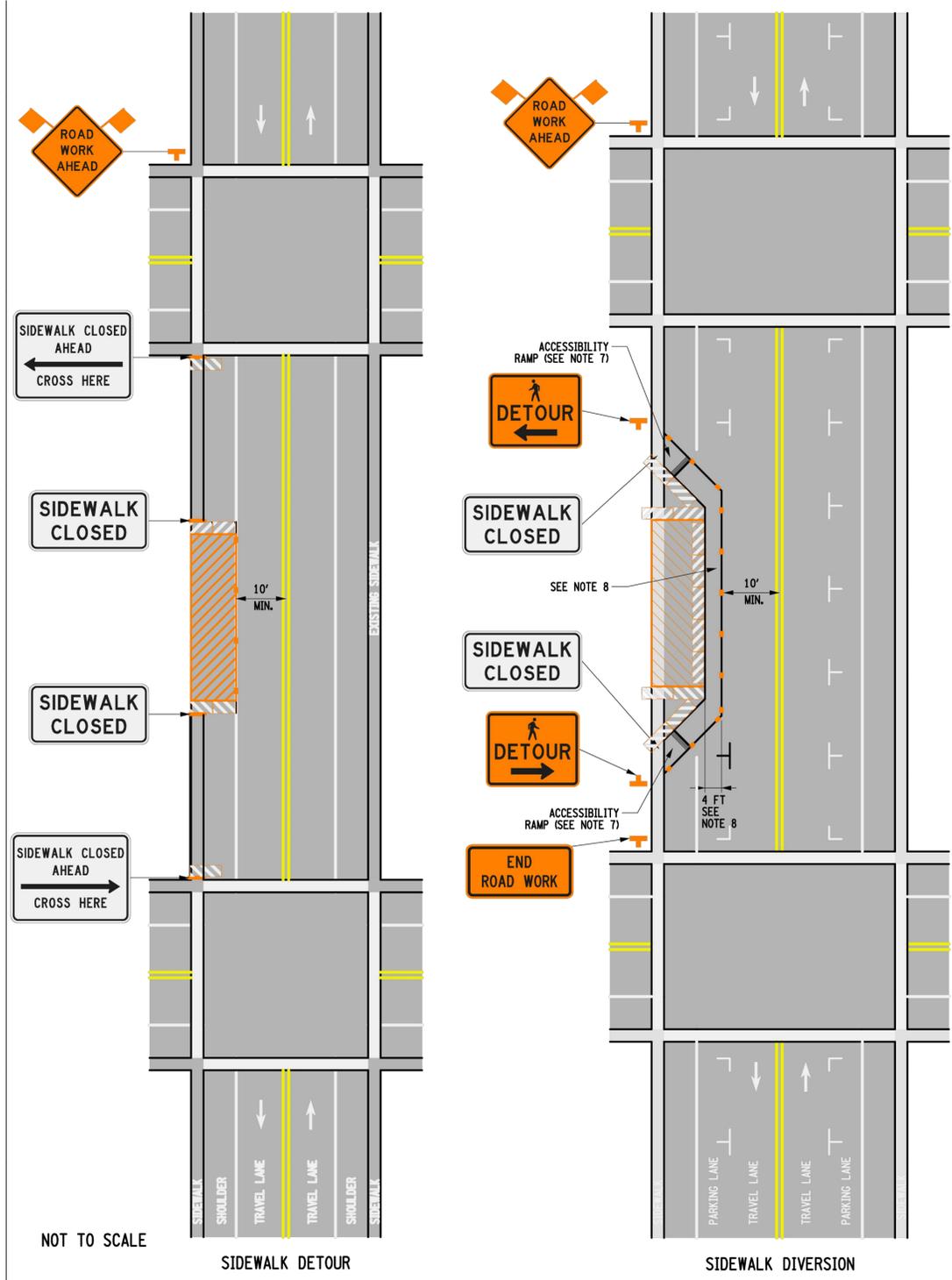
**WORK ZONE TRAFFIC CONTROL - 1**  
 NON-FREEWAY SHOULDER CLOSURE  
 SHORT TERM OPERATION  
 PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION  
 OF ASHFORD AVE AND GRANDVIEW AVE / STORM ST  
 VILLAGE OF DOBBS FERRY, NEW YORK

DRAWING NUMBER:

**WZTC-1**

SHEET: 7 OF 11

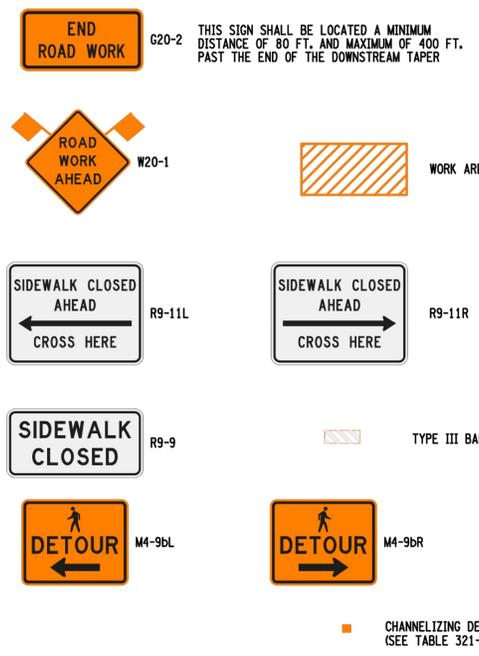
PDI: 230911-04-43 | DIR: G:\PROJECTS\23017\DWG\ROADWAY\23017\_GP\_4BUMPOUTS.DWG | DST: ROADWAY (PDF).PC3 | PST: \_ROADWAY - STANDARD.CTB | PSL: 1:1 | BY: LW



- NOTES:
1. SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
  2. ANY SIDEWALK DIVERSION MUST BE ADA COMPLIANT AND FOLLOW THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG). TEMPORARY RAMPS AND OTHER TEMPORARY PEDESTRIAN FACILITIES SHALL BE PAID FOR UNDER THE APPROPRIATE ITEM FOR HOT MIX ASPHALT (HMA) SIDEWALKS.
  3. PROWAG SECTION R205 AND MUTCD PART 6 SHALL APPLY TO ALL CONSTRUCTED TEMPORARY ALTERNATIVE PEDESTRIAN PATHWAYS.
  4. ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES, SUCH AS LANE/SHOULDER CLOSURE, ROAD NARROWS SIGNS (W5-4), OR NO PARKING SIGNS MAY BE USED TO CONTROL VEHICULAR TRAFFIC.
  5. TYPE III BARRICADES SHALL BE THE FULL WIDTH OF THE PATH BEING CLOSED.
  6. ACCESS TO BUSINESS ENTRANCES AND TRANSIT STOPS WILL NEED TO BE MAINTAINED. IF THE PEDESTRIAN FACILITY CURRENTLY HAS A TRANSIT STOP THAT WILL BE AFFECTED BY THE WORK ZONE, PRACTITIONERS SHOULD CONSULT THE TRANSIT AUTHORITIES THAT SERVICE THE AFFECTED STOP TO DETERMINE HOW TO ACCOMMODATE THE STOP.
  7. THE GRADE (RUNNING SLOPE), AND CROSS SLOPE OF A TEMPORARY CURB RAMP FOR DESIGN AND LAYOUT SHALL BE IN ACCORDANCE WITH STANDARD SHEET 608-01.
  8. A MINIMUM 4' CLEAR WIDTH SHALL BE MAINTAINED. THE MAXIMUM LENGTH OF A 4' WIDE PEDESTRIAN PATHWAY SHALL NOT EXCEED 200'.

WORK ZONE PROVISIONS	MAXIMUM DEVICE SPACING (CENTER TO CENTER)	MUTCD COMPLIANT CHANNELIZING DEVICE								
		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERMEDIATE TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
SHORT-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.	20 FT.	X	X	X	X			X	X	

NOTES: X= ALLOWED, BLANK = NOT ALLOWED



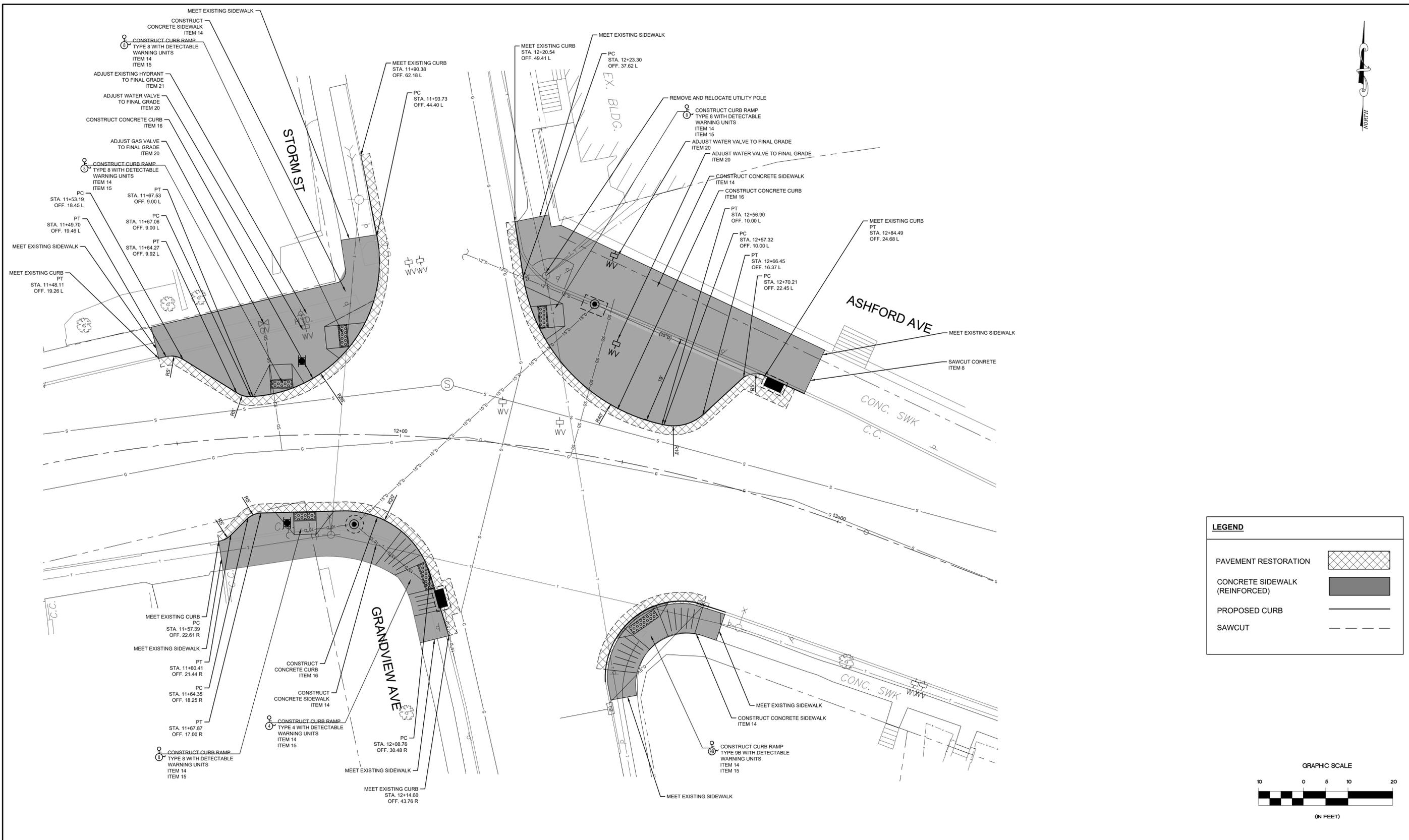
SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
R9-11L/R9-11R**	24x18	24x18
R9-9**	24x12	24x12
W20-1	36x36	48x48
M4-9BL/M4-9BR**	30x24	30x24
WARNING FLAG	18x18	18x18

\*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.  
\*\*SIGNS NOT FOR FREEWAY USE

NYS DOT STANDARD SHEET  
REF NO. 619-321

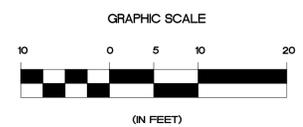
<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>DATE</th> <th>REVISION</th> <th>BY:</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	No.	DATE	REVISION	BY:					<p>THIS DOCUMENT IS THE SOLE PROPERTY OF N&amp;P ENGINEERS &amp; SURVEYORS, PLLC, AND HAS BEEN PREPARED AND DISSEMINATED FOR THE SOLE USE OF OUR CLIENT. THIS DOCUMENT AND ANY DATA CALCULATIONS, ESTIMATES OR DEPICTIONS CONTAINED HEREIN MAY NOT BE UTILIZED BY ANY THIRD-PARTY WITHOUT PRIOR WRITTEN CONSENT OF N&amp;P ENGINEERS &amp; SURVEYORS, PLLC, WHICH ASSUMES NO RISK OF LOSS OR LIABILITY RESULTING FROM ANY UNAUTHORIZED USE OR RELIANCE BY ANY THIRD-PARTY.</p>	<p>CONSULTANT:</p> <p><b>NELSON + POPE</b> engineers • architects • surveyors 70 Maxess Road, Melville, NY 11747 • 631.427.5665 • nelsonpoppe.com</p>	<p>CLIENT:</p> <p>VILLAGE OF DOBBS FERRY 112 MAIN STREET DOBBS FERRY, NEW YORK 10522</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>DWN BY:</td> <td>LW</td> </tr> <tr> <td>CHK'D BY:</td> <td>JD</td> </tr> <tr> <td>DATE:</td> <td>SEP 2023</td> </tr> <tr> <td>JOB No.:</td> <td>23017</td> </tr> <tr> <td>CADD:</td> <td>23017_GP_4BUMPOUTS</td> </tr> <tr> <td>SCALE:</td> <td>AS SHOWN</td> </tr> </table>	DWN BY:	LW	CHK'D BY:	JD	DATE:	SEP 2023	JOB No.:	23017	CADD:	23017_GP_4BUMPOUTS	SCALE:	AS SHOWN	<p>DRAWING TITLE:</p> <p><b>WORK ZONE TRAFFIC CONTROL - 2</b> TWO-LANE TWO-WAY ROADWAY SIDEWALK DETOUR/DIVERSION SHORT TERM OPERATION PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION OF ASHFORD AVE AND GRANDVIEW AVE / STORM ST VILLAGE OF DOBBS FERRY, NEW YORK</p>	<p>DRAWING NUMBER:</p> <p><b>WZTC-2</b></p> <p>SHEET: 8 OF 11</p>
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**LEGEND**

- PAVEMENT RESTORATION
- CONCRETE SIDEWALK (REINFORCED)
- PROPOSED CURB
- SAWCUT



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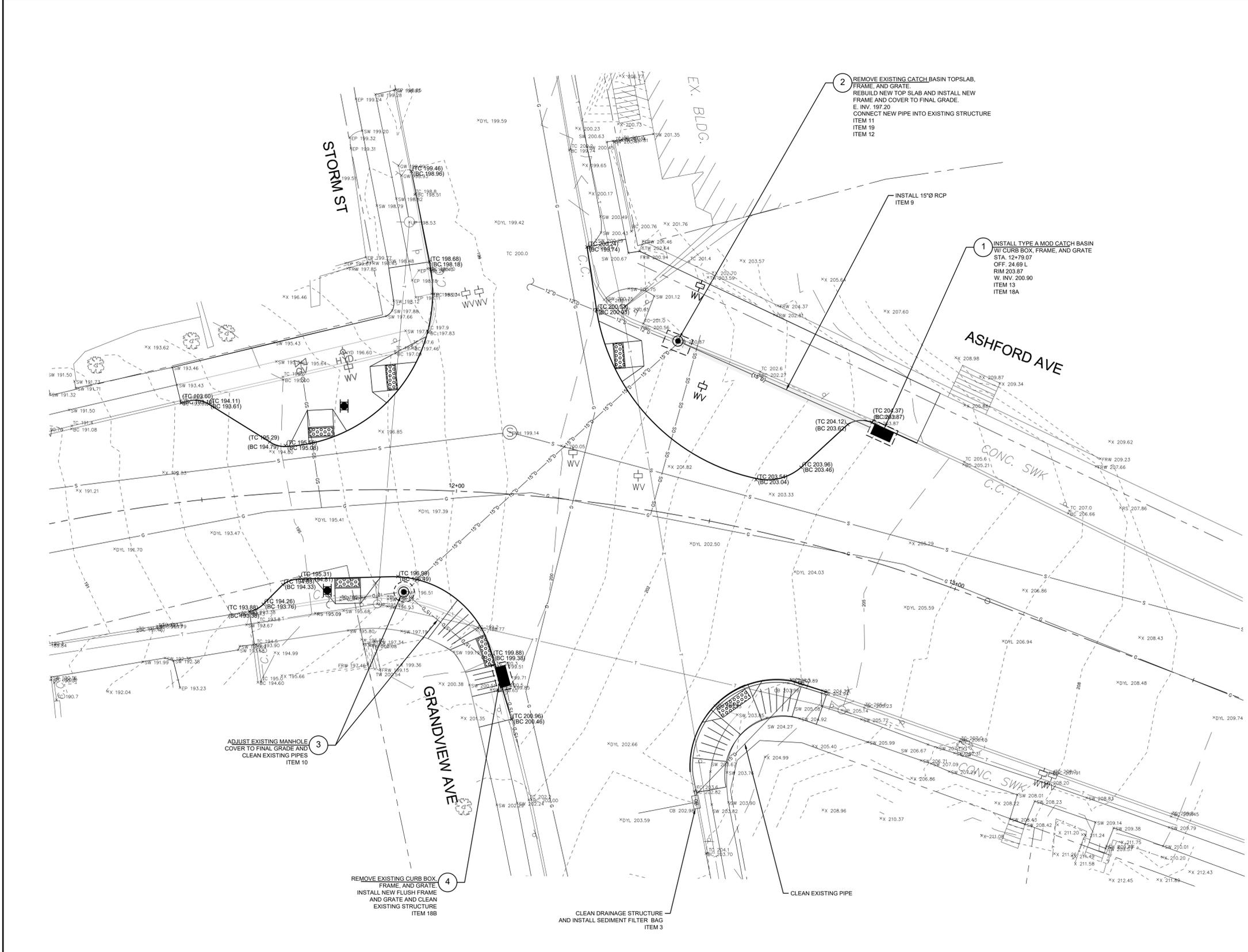
CONSTRUCTION PLAN - 1

PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION  
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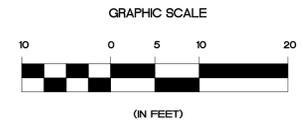
SHEET: 9 OF 11



NOTES:

- THE CONTRACTOR SHALL CLEAN EXISTING AND PROPOSED STRUCTURE AS NECESSARY.

LEGEND:	
* X/BRICK 99.99	EXISTING SURVEY SPOT ELEVATION
- - - 202 - - -	EXISTING MINOR CONTOUR
- - - 205 - - -	EXISTING MAJOR CONTOUR
* (TC 99.99)	PROPOSED SPOT ELEVATION
	PROPOSED DRAINAGE CATCH BASIN TYPE A-MOD
(12\"D)	PROPOSED DRAINAGE PIPE
	PROPOSED DRAINAGE MANHOLE



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PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION OF ASHFORD AVE AND GRANDVIEW AVE / STORM ST VILLAGE OF DOBBS FERRY, NEW YORK

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