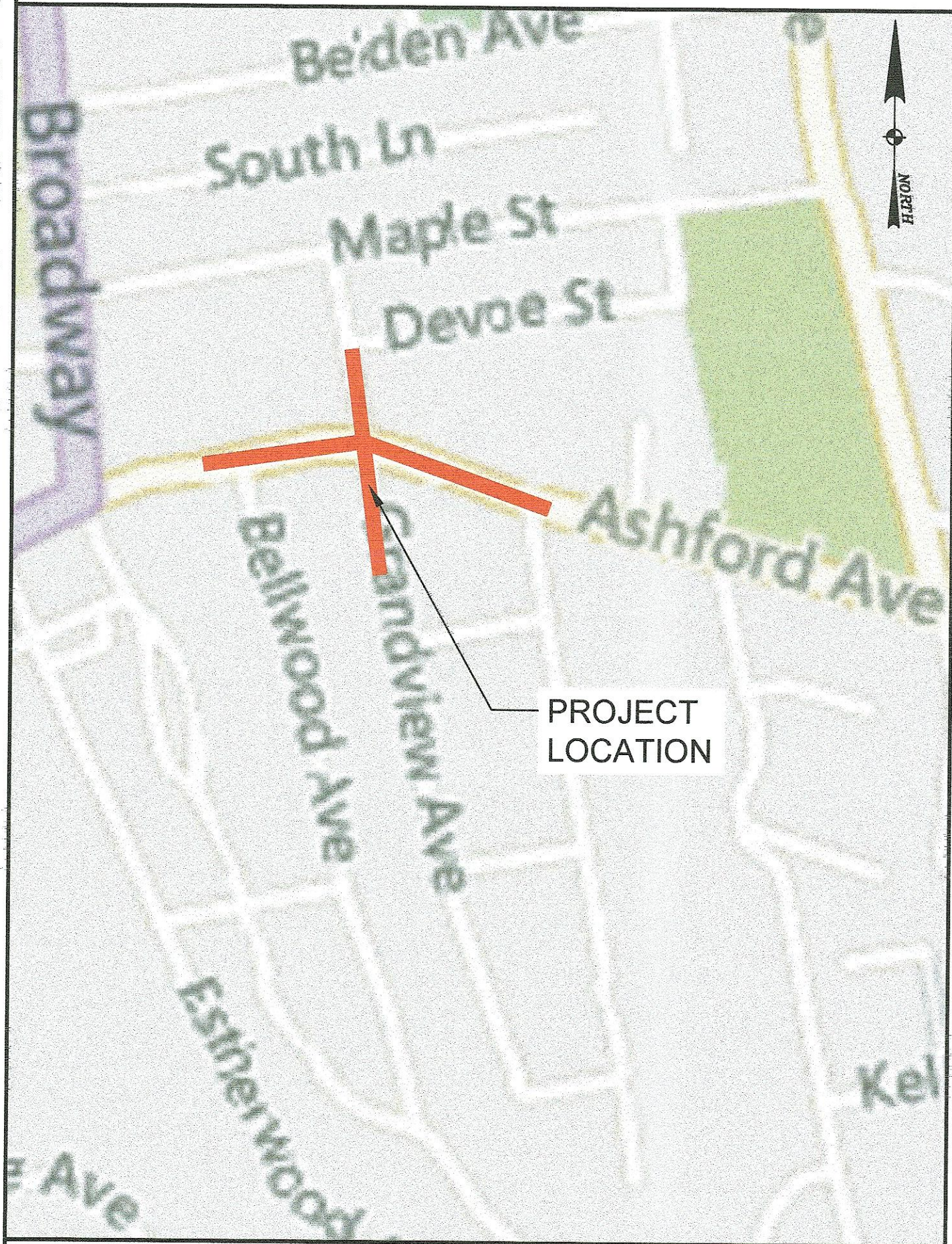


VILLAGE OF DOBBS FERRY WESTCHESTER COUNTY, NEW YORK

PLANS FOR PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION OF ASHFORD AVE AND GRANDVIEW AVE/STORM ST



LOCATION MAP
N.T.S.

SEPTEMBER 2023

N+ P NO. 23017

IT IS A VIOLATION OF NEW YORK STATE EDUCATION LAW ARTICLE 145, PROFESSIONAL ENGINEERING AND LAND SURVEYING, SECTION 7209 FOR ANY PERSON, UNLESS HE IS ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE SEAL OF AN ENGINEER OR LAND SURVEYOR IS ALTERED, THE ALTERING ENGINEER OR LAND SURVEYOR SHALL AFFIX TO THE ITEM HIS SEAL AND THE NOTATION "ALTERED BY" FOLLOWED BY HIS SIGNATURE AND THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

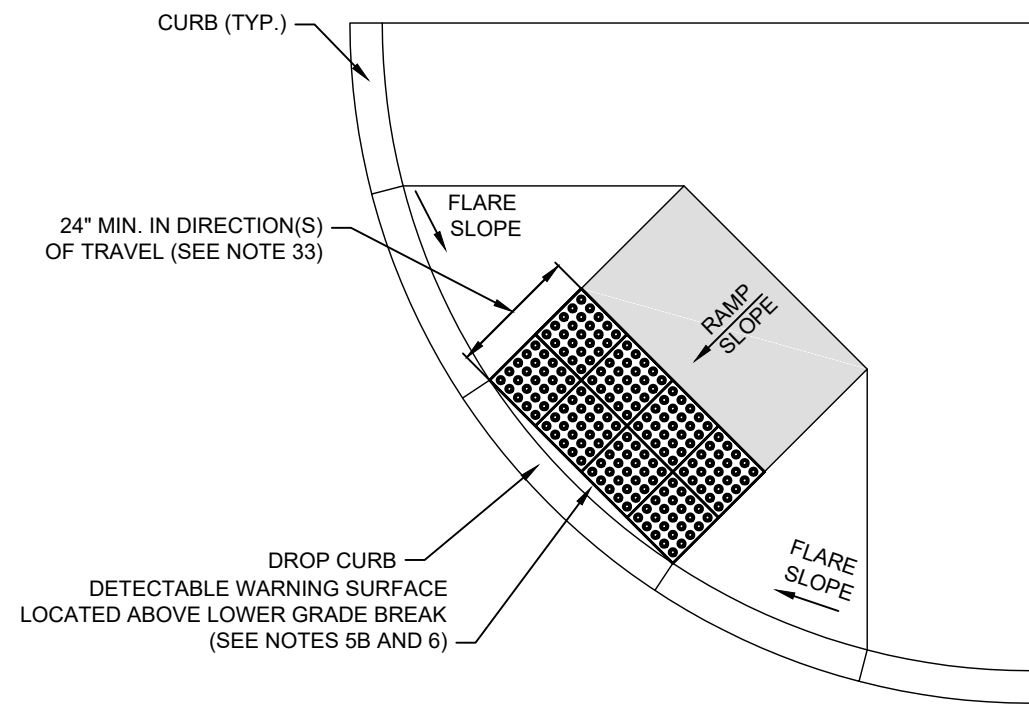


NELSON + POPE
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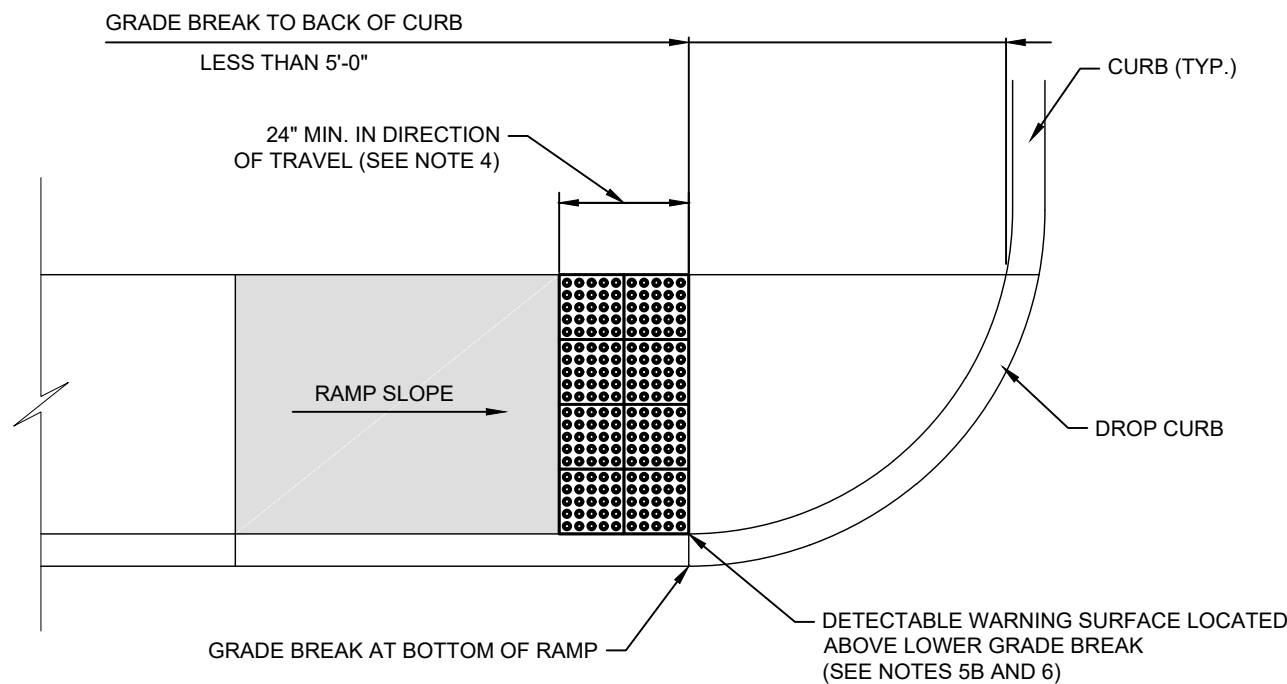
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PDT:230913-12:33 | DIR:C:\PROJECTS\23017\DWG\ROADWAY\23017_COVDWG | DST:ARCH HP 17200 PLOTTER | PSE:MAP 2009_MIT-LARGE-SCALE TOPO - COLOR:CTB | PSL:1:1 | BY:LLW

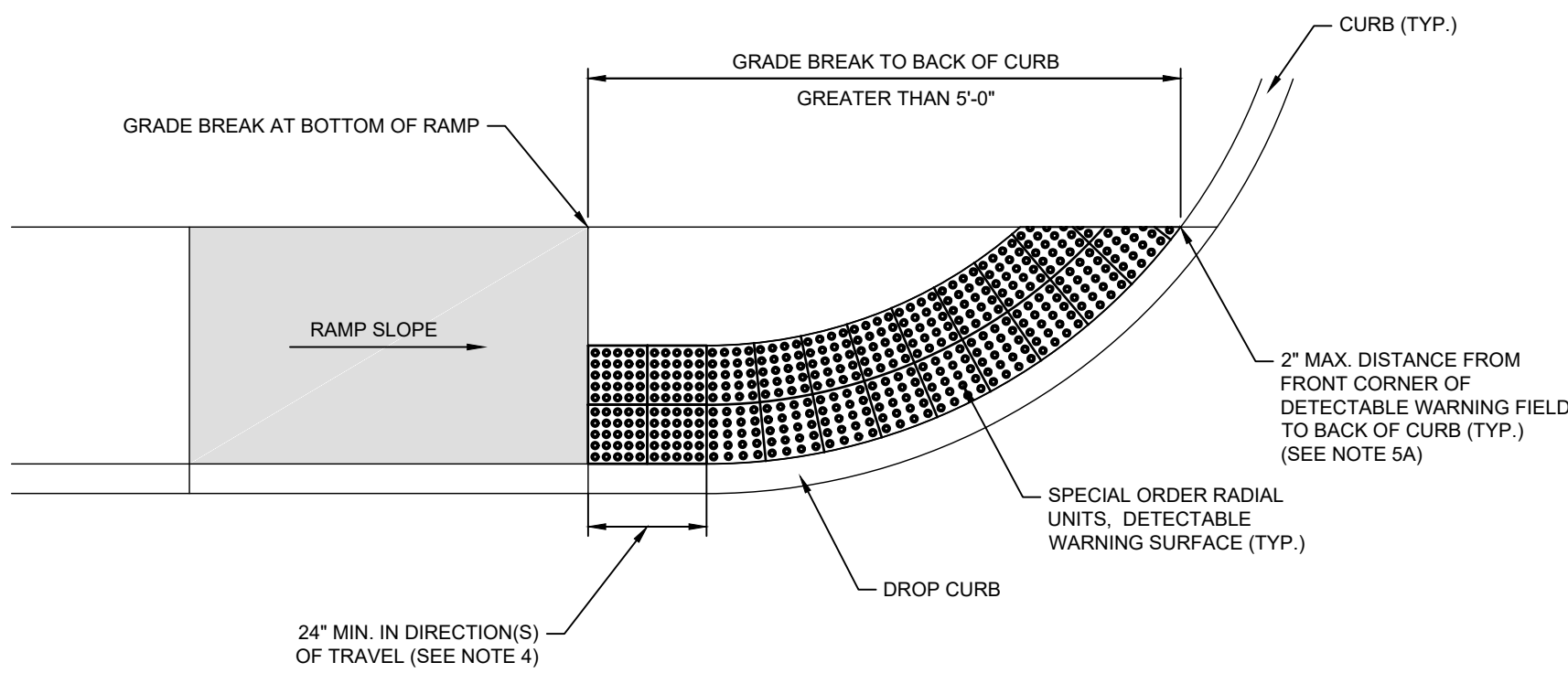
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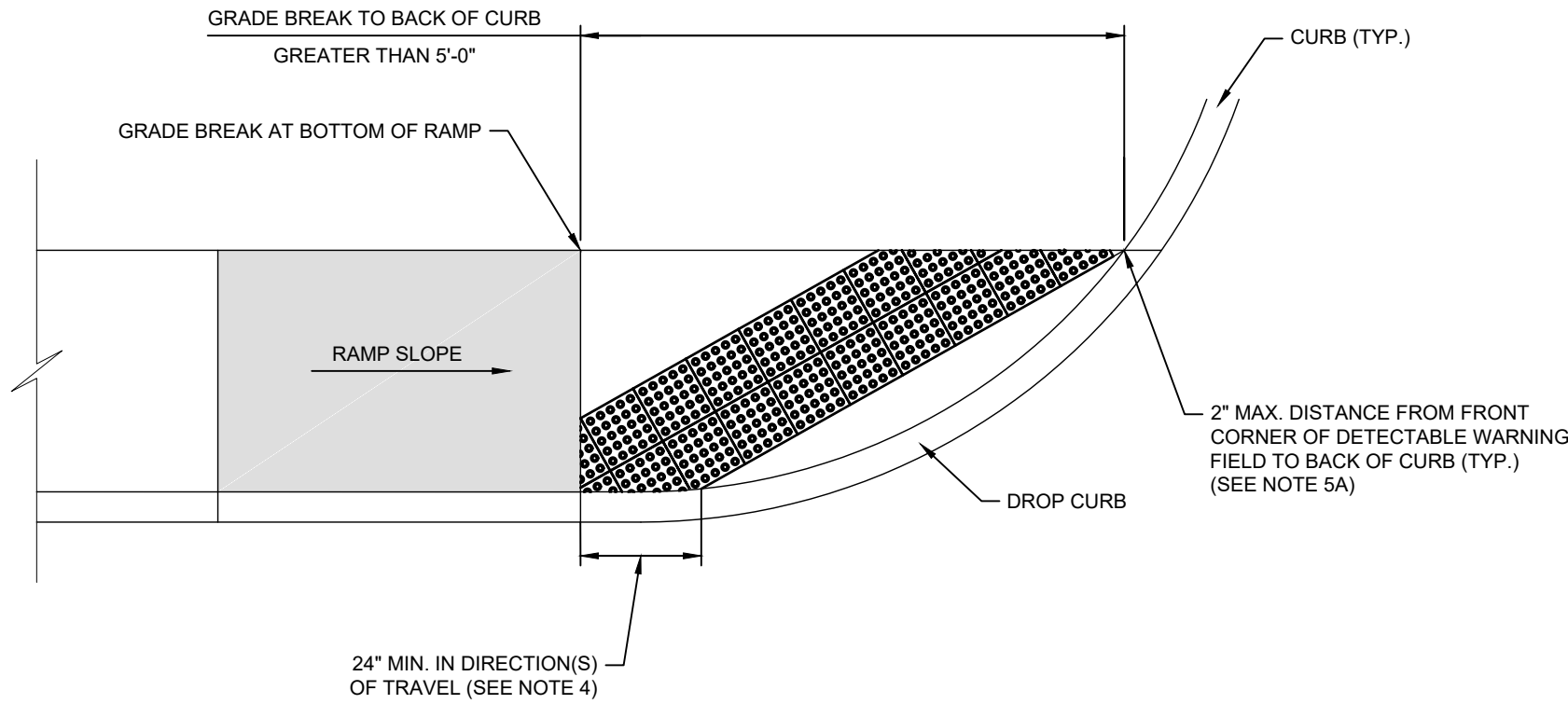
DETECTABLE WARNING SURFACE PLACEMENT - OPTION 1



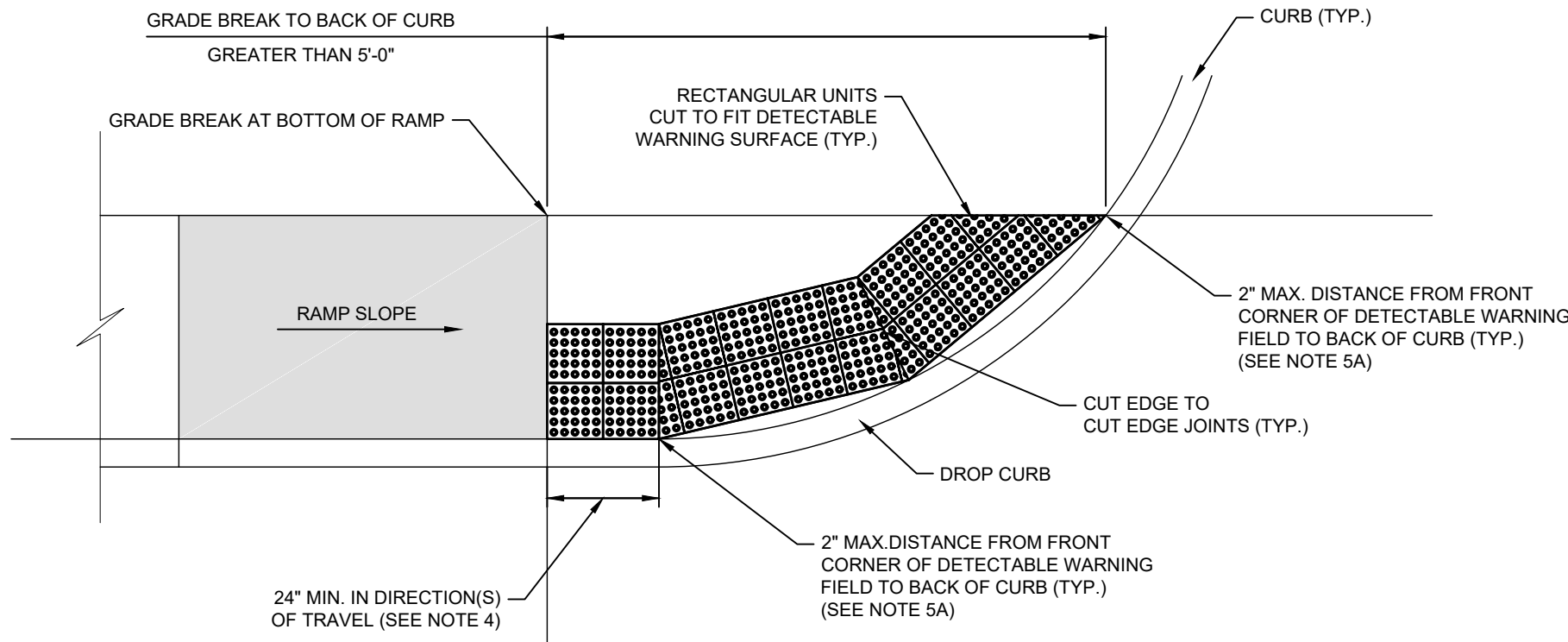
DETECTABLE WARNING SURFACE PLACEMENT - OPTION 2



DETECTABLE WARNING SURFACE PLACEMENT - OPTION 3



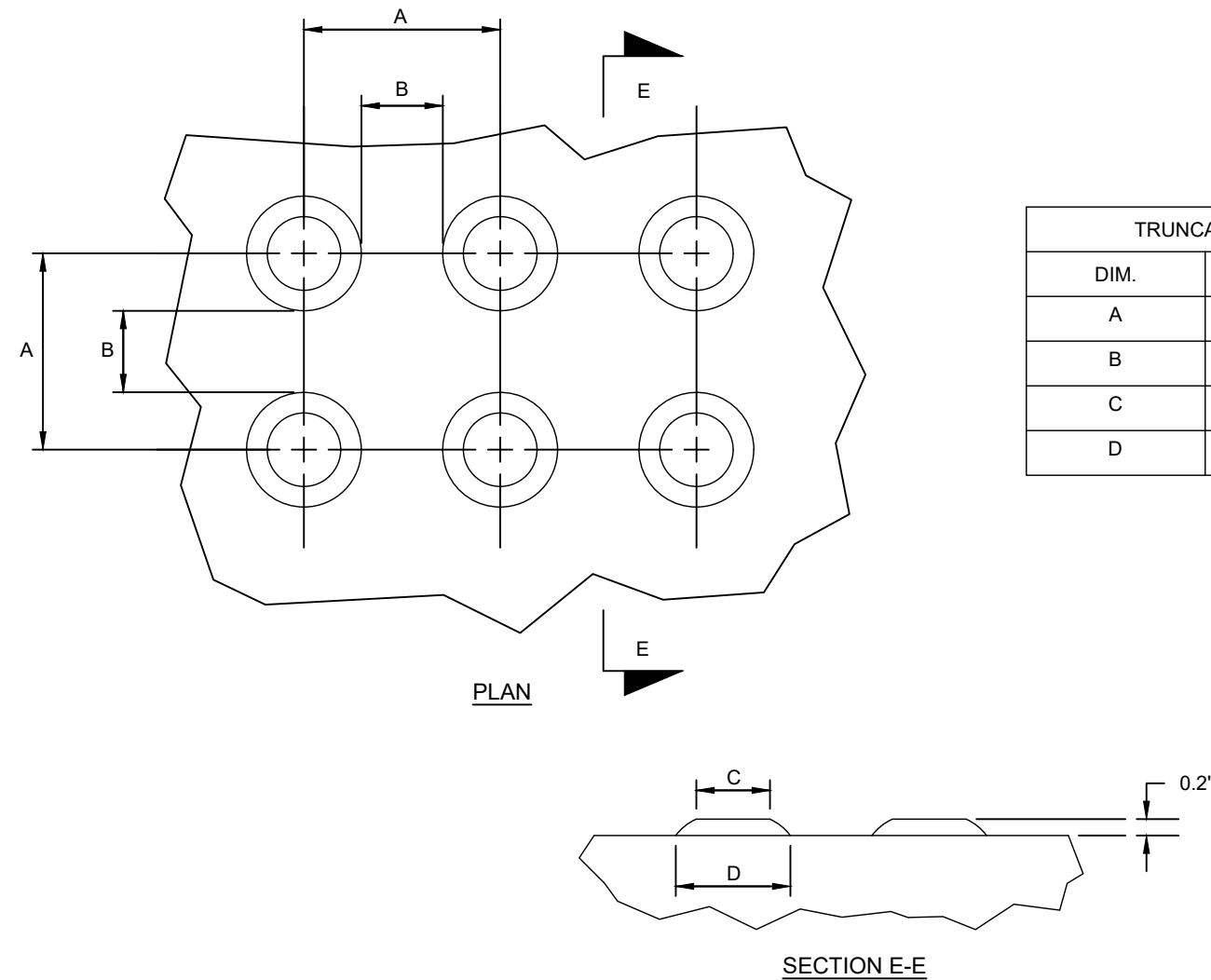
DETECTABLE WARNING SURFACE PLACEMENT - OPTION 4



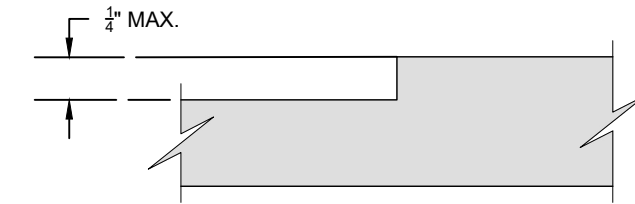
DETECTABLE WARNING SURFACE PLACEMENT - OPTION 5

DETECTABLE WARNING NOTES:

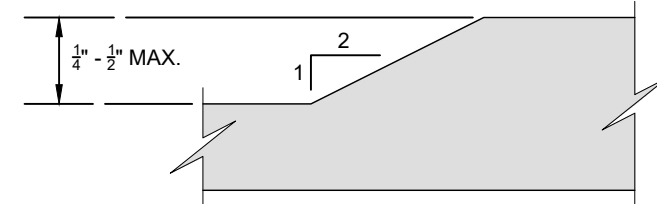
- DETECTABLE WARNING SURFACES (DWS) SHALL BE PROVIDED AT THE FOLLOWING LOCATIONS OF PEDESTRIAN ACCESS ROUTES:
 - CURB RAMPS ARE BLENDED TRANSITIONS AT PEDESTRIAN STREET CROSSINGS.
 - PEDESTRIAN REFUGE ISLANDS (WHERE THE LENGTH OF THE PEDESTRIAN ACCESS ROUTE ACROSS THE REFUGE ISLAND IS GREATER THAN OR EQUAL TO 6 FEET).
 - PEDESTRIAN AT-GRADE RAIL CROSSINGS NOT LOCATED WITHIN A STREET OR HIGHWAY.
- DETECTABLE WARNING SURFACES SHALL BE PROVIDED WHERE THE PEDESTRIAN ACCESS ROUTE CROSSES DRIVEWAYS WITH SIGNAL, YIELD OR STOP CONTROL. DETECTABLE WARNING SURFACES SHALL NOT BE PROVIDED AT CROSSINGS OF UNCONTROLLED DRIVEWAYS.
- WITH THE EXCEPTION OF THE "DETECTABLE WARNING SURFACE TRUNCATED DOME DETAILS" DETECTABLE WARNING DOMES ON THIS SHEET ARE NOT DEPICTED TO SCALE.
- DETECTABLE WARNING FIELDS SHALL EXTEND 24" MINIMUM IN THE DIRECTION OF PEDESTRIAN TRAVEL ACROSS THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE, EXCLUDING AND FLARED SIDES.
- SOME DETECTABLE WARNING PRODUCTS REQUIRE A CONCRETE BORDER FOR PROPER INSTALLATION. BORDERS CANNOT BE INCLUDED AS PART OF THE 24" MINIMUM DIMENSION DESCRIBED IN NOTE 4.
 - WHEN PLACED AT THE BACK OF CURB, DETECTABLE WARNING FIELDS SHOULD BE PLACED AS CLOSE TO THE BACK OF CURB AS PRACTICABLE. BOTH FRONT CORNERS OF THE DETECTABLE WARNING FIELD SHALL NOT BE LOCATED MORE THAN 2" FROM THE BACK OF CURB. WHERE THE BACK OF THE CURB EDGE IS TOOLED TO PROVIDE A RADIUS, THE BORDER DIMENSION SHALL BE MEASURED FROM THE INSIDE EDGE OF THE CURB RADIUS. WHERE CURB IS NOT USED, THE EDGE OF PAVEMENT SHALL BE SUBSTITUTED FOR THE BACK OF CURB FOR PLACEMENT OF DETECTABLE WARNINGS.
 - WHEN PLACED ABOVE THE LOWER GRADE BREAK OF A CURB RAMP, DETECTABLE WARNING UNITS SHOULD BE PLACED AS CLOSE TO THE JOINT AS PRACTICABLE. BOTH FRONT CORNERS OF RECTILINEAR DETECTABLE WARNING FIELDS SHALL NOT BE LOCATED MORE THAN 2" FROM THE JOINT.
 - WHEN RADIAL DWS UNITS ARE PLACED AT THE BACK OF CURB, THE FRONT EDGE OF THE DWS FIELD SHOULD BE AS CLOSE AS POSSIBLE TO THE BACK OF CURB, I.E., THE RADIUS OF THE FRONT OF THE DWS FIELD SHOULD MATCH THE RADIUS ALONG THE BACK OF CURB AS CLOSELY AS POSSIBLE. THE OUTSIDE CORNERS OF THE DWS FIELD MUST BE LOCATED NO MORE THAN 2 INCHES FROM THE BACK OF CURB.
- ON SLOPES OF 5% OR GREATER, THE ROWS OF DOMES SHALL BE ALIGNED TO BE PERPENDICULAR OR RADIAL TO THE LOWER GRADE BREAK ON THE RAMP RUN. WHERE DOMES ARE ARRAYED RADIALY, THEY MAY DIFFER IN DIAMETER AND CENTER-TO-CENTER SPACING WITHIN THE RANGES SPECIFIED ON THIS SHEET. DOME ALIGNMENT THAT IS PERPENDICULAR OR RADIAL TO THE LOWER GRADE BREAK IS NOT REQUIRED ON SLOPES OF LESS THAN 5%.
- THE DETECTABLE WARNING FIELD SHALL BE THE COLOR SPECIFIED IN THE CONTRACT DOCUMENTS OR MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS. DETECTABLE WARNING SURFACES CONTRAST VISUALLY WITH ADJACENT GUTTER, STREET OR HIGHWAY, OR PEDESTRIAN ACCESS ROUTE SURFACE, EITHER LIGHT-ON-DARK OR DARK -ON-LIGHT.



DETECTABLE WARNING SURFACE (DWS) TRUNCATED DOME DETAILS



VERTICAL SURFACE DISCONTINUITIES
SEE NOTE 5 ON SHEET 1 OF 12



No.	DATE	REVISION	BY:

SEAL:

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CONSULTANT:



NELSON + POPE
engineers • architects • surveyors
70 Maxess Road, Melville, NY 11747 • 631.427.5665 • nelsonpoppe.com

CLIENT:

VILLAGE OF DOBBS FERRY
112 MAIN STREET
DOBBS FERRY, NEW YORK 10522

DWN. BY:	LW
CHKD BY:	JD
DATE:	SEP 2023
JOB No.:	23017
CADD:	23017_DET.DWG
SCALE:	AS SHOWN

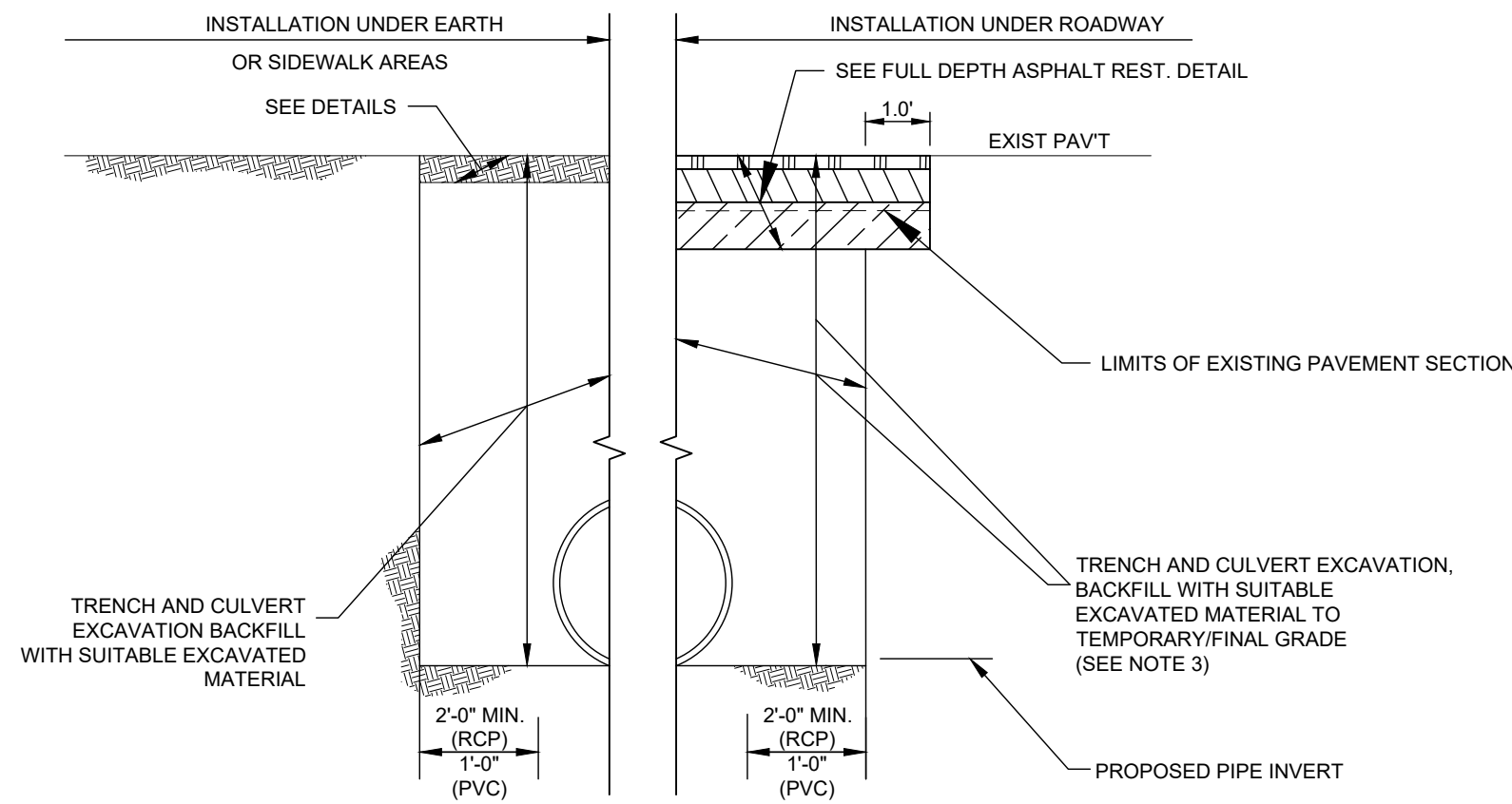
DRAWING TITLE:

MISCELLANEOUS DETAILS - 1
FOR
PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION
OF ASHFORD AVE AND GRANDVIEW AVE / STORM ST
VILLIAGE OF DOBBS FERRY, NY 10522

DRAWING NUMBER:

MD - 1

SHEET: 2 OF 11



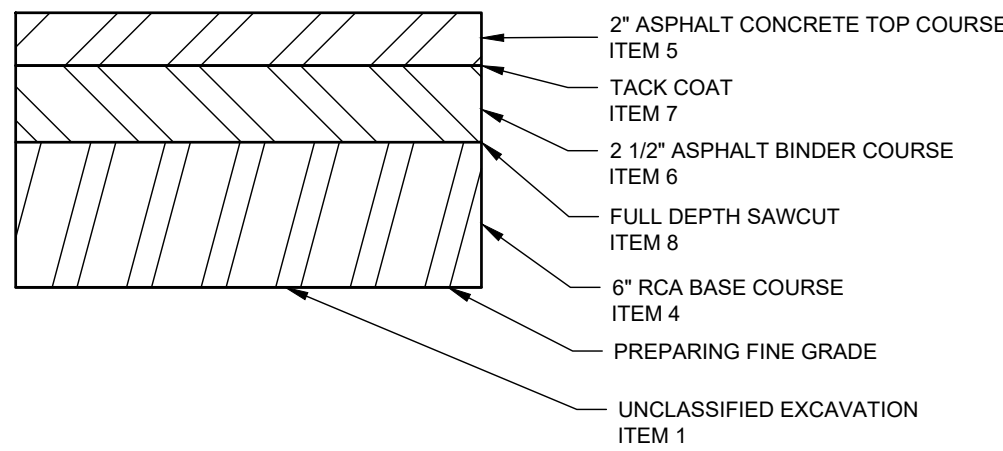
DRAINAGE PIPE INSTALLATION

ITEM 9

NOT TO SCALE

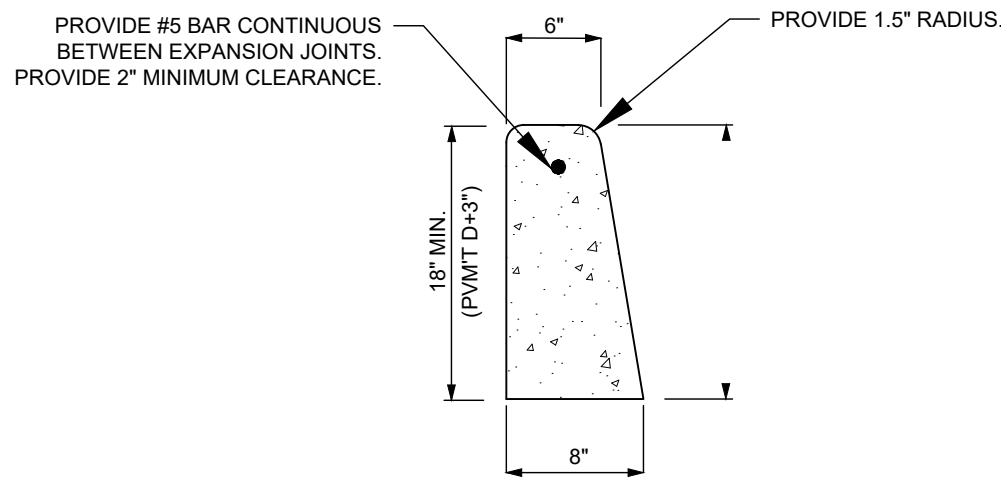
DRAINAGE TRENCHING NOTES:

- ALL REINFORCED CONCRETE PIPE TO BE INSTALLED UNDER THIS CONTRACT SHALL HAVE A MINIMUM TRENCH WIDTH OF 4'-0" + PIPE DIAMETER FOR PROPER INSTALLATION, BACKFILL AND COMPACTION PROCEDURES TO TAKE PLACE.
- ALL POLYVINYL CHLORIDE (PVC) PIPE INSTALLATIONS SHALL ONLY TAKE PLACE WITHIN THE SIDEWALK AREA, AS DEPICTED ON THE CONSTRUCTION PLANS HAVING A TRENCH WIDTH OF 2'-0" + PIPE DIAMETER.
- TEMPORARY ASPHALT MAY BE PLACED BETWEEN THE BACKFILL AND TEMPORARY/FINAL GRADE, ONLY IF DETERMINED NECESSARY BY THE ENGINEER IN CHARGE, MAX 3 INCHES, TO BE PAID FOR UNDER 36T



FULL DEPTH ASPHALT RESTORATION DETAIL

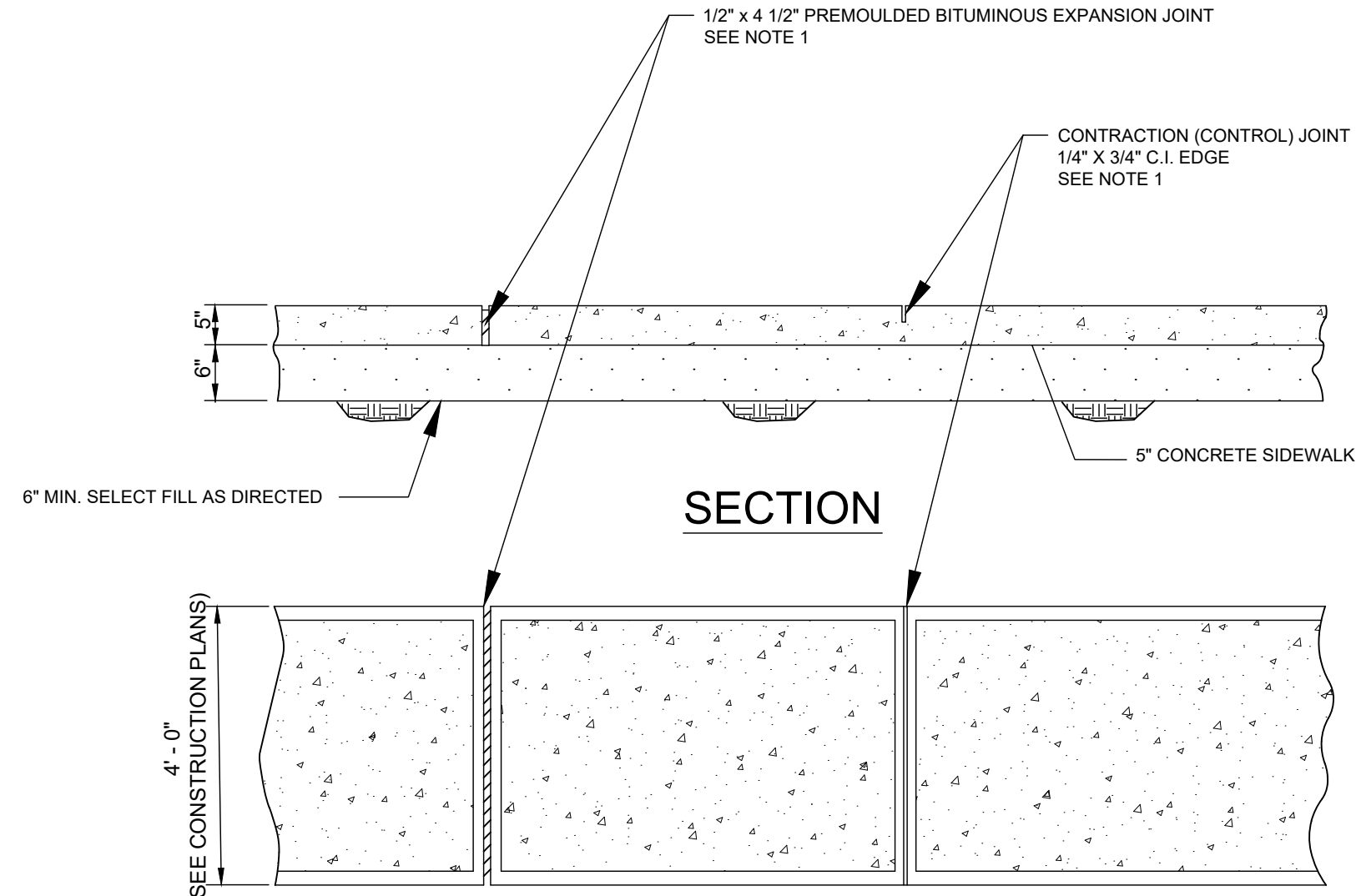
NOT TO SCALE



CONCRETE CURB DETAIL

ITEM 16

NOT TO SCALE



CONCRETE SIDEWALK

ITEM 14

NOT TO SCALE

SIDEWALK NOTES:

- CONTROL JOINTS ARE TO BE PLACED EVERY 5'-0" AND EXPANSION JOINTS ARE TO BE PLACED EVERY 20', OR AS DIRECTED BY THE ENGINEER.
- ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED 5" THICK.
- ALL SIDEWALKS IN INTERSECTIONS AND IN SMALL RADII SHALL BE PLACED MONOLITHICALLY AND SHALL BE 5 INCHES THICK. UNREINFORCED CONCRETE FROM THE PC TO THE PT. THE AREA TO BE PAID FOR THE SIDEWALK ITEM SHALL NOT INCLUDE THE CURB AND GUTTER.

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No.	DATE	REVISION	BY:							
										SHEET: 4 OF 11

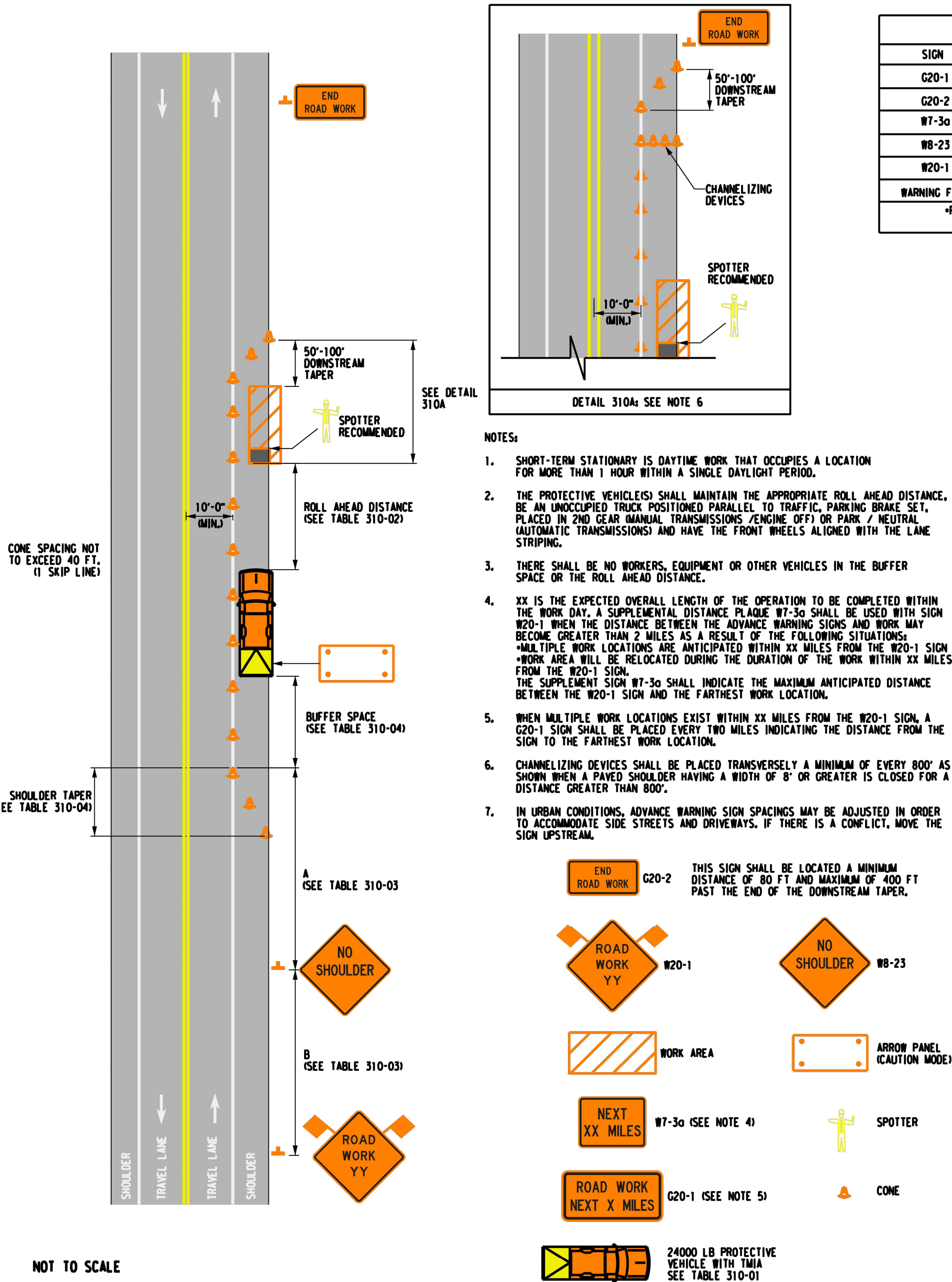


NOTES:

1. WINDOW TO BE CAST IN STRUCTURE AS REQUIRED.
KNOCK-OUT PANELS WILL NOT BE PERMITTED.
2. REBARS: $f_s = 40,000 \text{ PSI}$ ASTM A-615
3. W.W.M.: $6" \times 6" - 4/4$
 $f_s = 60,000 \text{ PSI}$, ASTM A-185
PROVIDE 1 $\frac{1}{2}"$ COVER AT ENDS
OF VERTICAL WIRES.
1" RADIUS BENDS AT CORNERS.
4. CONCRETE: CLASS "A" AS PER
NASSAU COUNTY SPECIFICATIONS.
5. FRAME "D": TYPE 1 & 2 = $2'-1 \frac{1}{2}"$
TYPE 'A' = $2'-7 \frac{1}{2}"$
6. UTILIZE (2) LIFT HOLES FOR HANDLING.


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No.	DATE	REVISION	BY:							SHEET: 6 OF 11

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NOTES:

1. SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
2. THE PROTECTIVE VEHICLE(S) SHALL MAINTAIN THE APPROPRIATE ROLL AHEAD DISTANCE. BE AN UNOCCUPIED TRUCK POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS /ENGINE OFF) OR PARK / NEUTRAL (AUTOMATIC TRANSMISSIONS) AND HAVE THE FRONT WHEELS ALIGNED WITH THE LANE STRIPING.
3. THERE SHALL BE NO WORKERS, EQUIPMENT OR OTHER VEHICLES IN THE BUFFER SPACE OR THE ROLL AHEAD DISTANCE.
4. XX IS THE EXPECTED OVERALL LENGTH OF THE OPERATION TO BE COMPLETED WITHIN THE WORK DAY. A SUPPLEMENTAL DISTANCE PLaque W7-3a SHALL BE USED WITH SIGN W20-1 WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK MAY BECOME GREATER THAN 2 MILES AS A RESULT OF THE FOLLOWING SITUATIONS:
• MULTIPLE WORK LOCATIONS ARE ANTICIPATED WITHIN XX MILES FROM THE W20-1 SIGN
• WORK AREA WILL BE RELOCATED DURING THE DURATION OF THE WORK WITHIN XX MILES FROM THE W20-1 SIGN.
THE SUPPLEMENT SIGN W7-3a SHALL INDICATE THE MAXIMUM ANTICIPATED DISTANCE BETWEEN THE W20-1 SIGN AND THE FARTHEST WORK LOCATION.
5. WHEN MULTIPLE WORK LOCATIONS EXIST WITHIN XX MILES FROM THE W20-1 SIGN, A G20-1 SIGN SHALL BE PLACED EVERY TWO MILES INDICATING THE DISTANCE FROM THE SIGN TO THE FARTHEST WORK LOCATION.
6. CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.
7. IN URBAN CONDITIONS, ADVANCE WARNING SIGN SPACINGS MAY BE ADJUSTED IN ORDER TO ACCOMMODATE SIDE STREETS AND DRIVEWAYS. IF THERE IS A CONFLICT, MOVE THE SIGN UPSTREAM.




END ROAD WORK G20-2


THIS SIGN SHALL BE LOCATED A MINIMUM DISTANCE OF 80 FT AND MAXIMUM OF 400 FT PAST THE END OF THE DOWNSTREAM TAPER.




ROAD WORK YY W20-1




NO SHOULDER W8-23




WORK AREA




ARROW PANEL (CAUTION MODE)




NEXT XX MILES W7-3a (SEE NOTE 4)




SPOTTER



ROAD WORK NEXT X MILES G20-1 (SEE NOTE 5)



CONE



24000 LB PROTECTIVE VEHICLE WITH TMIA SEE TABLE 310-01

TABLE 310-05: REQUIRED SIGN SIZES*		
SIGN	NON-FREEWAY	FREEWAY
G20-1	36x18	48x24
G20-2	36x18	48x24
W7-3a	24x18	36x30
W8-23	36x36	48x48
W20-1	36x36	48x48
WARNING FLAG	18x18	18x18
*FREEWAY SIZES MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.		

TABLE 310-01: PROTECTIVE VEHICLE REQUIREMENTS				
CLOSURE TYPE	ROAD TYPE & SPEED	NON-FREEWAY		
		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH
LANE CLOSURE OR ENCROACHMENT	EXPOSURE CONDITIONS ¹			
	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P, TMIA	P
SHOULDER CLOSURE OR ENCROACHMENT	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	P	SEE NOTE 2
	WORKERS ON FOOT OR VEHICLE EXPOSED TO TRAFFIC	P, TMIA	P	P
	OTHER HAZARDS NO WORKERS EXPOSED	P, TMIA	P	SEE NOTE 2
LEGEND				
P: PROTECTIVE VEHICLE REQUIRED FOR EACH CLOSED LANE & EACH CLOSED PAVED SHOULDER 8' OR WIDER. IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE PROTECTIVE VEHICLE SHALL BE REPOSITIONED ACCORDINGLY				
TMIA: TMIA REQUIRED				
NOTES:				
1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT				
2. EITHER A PROTECTIVE VEHICLE OR THE STANDARD BUFFER SPACE SHALL BE PROVIDED				

TABLE 310-02: ROLL AHEAD DISTANCE		
ROLL AHEAD DISTANCE (FT.) ^a OF SKIP LINES FOR VEHICLES		
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	STATIONARY OPERATION	
	MIN	MAX
≥ 55	120/3	200/5
45 - 50	80/2	160/4
≤ 40	40/1	120/3

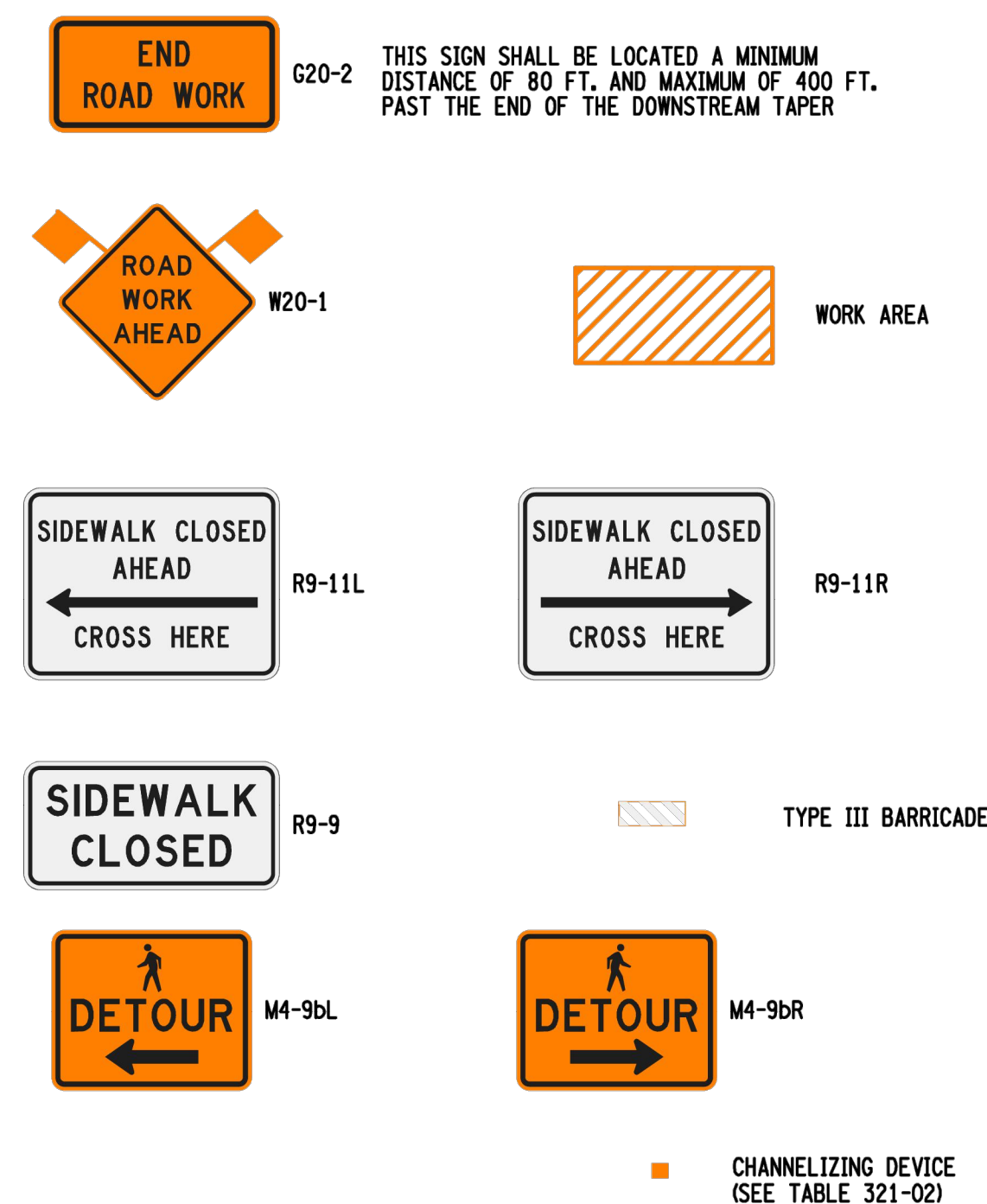
TABLE 310-03: ADVANCE WARNING SIGN SPACING				
ROAD TYPE	DISTANCE BETWEEN SIGNS		SIGN LEGEND	
	A (FT.)	B (FT.)	XX	YY
URBAN (≤ 30 MPH ^a)	100	100	AHEAD	AHEAD
URBAN (35-40 MPH ^a)	200	200	AHEAD	AHEAD
URBAN (≥ 45 MPH ^a)	350	350	1000 FT.	AHEAD
RURAL	500	500	1500 FT.	1000 FT.
^a PRECONSTRUCTION POSTED SPEED LIMIT				

TABLE 310-04: LONGITUDINAL BUFFER SPACE AND TAPER LENGTHS						
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	LONGITUDINAL BUFFER SPACE DISTANCE (FT.) ^a OF SKIP LINES	TAPER LENGTH: L (FT.) ^a * OF SKIP LINES/ ^a OF CHANNELIZING DEVICES			SHOULDER TAPER LENGTH: L/3 (FT.) ^a * OF SKIP LINES/ ^a OF CHANNELIZING DEVICES	
		FOR LANE WIDTH IN FT. (LATERAL SHIFT OF TRAFFIC FLOW PATH)			FOR SHOULDER WIDTH	
		10	11	12	≤ 4 FT.	5 - 7 FT.
25	155/4	120/3/4	120/3/4	120/3/4	40/1/2	40/1/2
30	200/5	160/4/5	160/4/5	200/5/6	40/1/2	40/1/2
35	250/6	200/5/6	240/6/7	240/6/7	40/1/2	80/2/3
40	305/8	280/7/8	320/8/9	320/8/9	40/1/2	80/2/3
45	360/9	440/11/12	520/13/14	560/14/15	80/2/3	80/2/3
50	425/11	520/13/14	560/14/15	600/15/16	80/2/3	120/3/4
55	495/13	560/14/15	600/15/16	680/17/18	80/2/3	160/4/5

NOTE:
1. THE CONTRACTOR SHALL SUBMIT SITE SPECIFIC WORK ZONE TRAFFIC CONTROL SCHEMES TO VILLAGE FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION. A FULL CLOSURE WILL NOT BE ACCEPTABLE.

NYS DOT STANDARD SHEET
REF NO. 619-310

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No.	DATE	REVISION	BY:						



- NOTES:
1. SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
 2. ANY SIDEWALK DIVERSION MUST BE ADA COMPLIANT AND FOLLOW THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG). TEMPORARY RAMPS AND OTHER TEMPORARY PEDESTRIAN FACILITIES SHALL BE PAID FOR UNDER THE APPROPRIATE ITEM FOR HOT MIX ASPHALT (HMA) SIDEWALKS.
 3. PROWAG SECTION R205 AND MUTCD PART 6 SHALL APPLY TO ALL CONSTRUCTED TEMPORARY ALTERNATIVE PEDESTRIAN PATHWAYS.
 4. ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES, SUCH AS LANE/SHOULDER CLOSURE, ROAD NARROWS SIGNS (W5-4), OR NO PARKING SIGNS MAY BE USED TO CONTROL VEHICULAR TRAFFIC.
 5. TYPE III BARRICADES SHALL BE THE FULL WIDTH OF THE PATH BEING CLOSED.
 6. ACCESS TO BUSINESS ENTRANCES AND TRANSIT STOPS WILL NEED TO BE MAINTAINED. IF THE PEDESTRIAN FACILITY CURRENTLY HAS A TRANSIT STOP THAT WILL BE AFFECTED BY THE WORK ZONE, PRACTITIONERS SHOULD CONSULT THE TRANSIT AUTHORITIES THAT SERVICE THE AFFECTED STOP TO DETERMINE HOW TO ACCOMMODATE THE STOP.
 7. THE GRADE (RUNNING SLOPE), AND CROSS SLOPE OF A TEMPORARY CURB RAMP FOR DESIGN AND LAYOUT SHALL BE IN ACCORDANCE WITH STANDARD SHEET 608-01.
 8. A MINIMUM 4' CLEAR WIDTH SHALL BE MAINTAINED. THE MAXIMUM LENGTH OF A 4' WIDE PEDESTRIAN PATHWAY SHALL NOT EXCEED 200'.

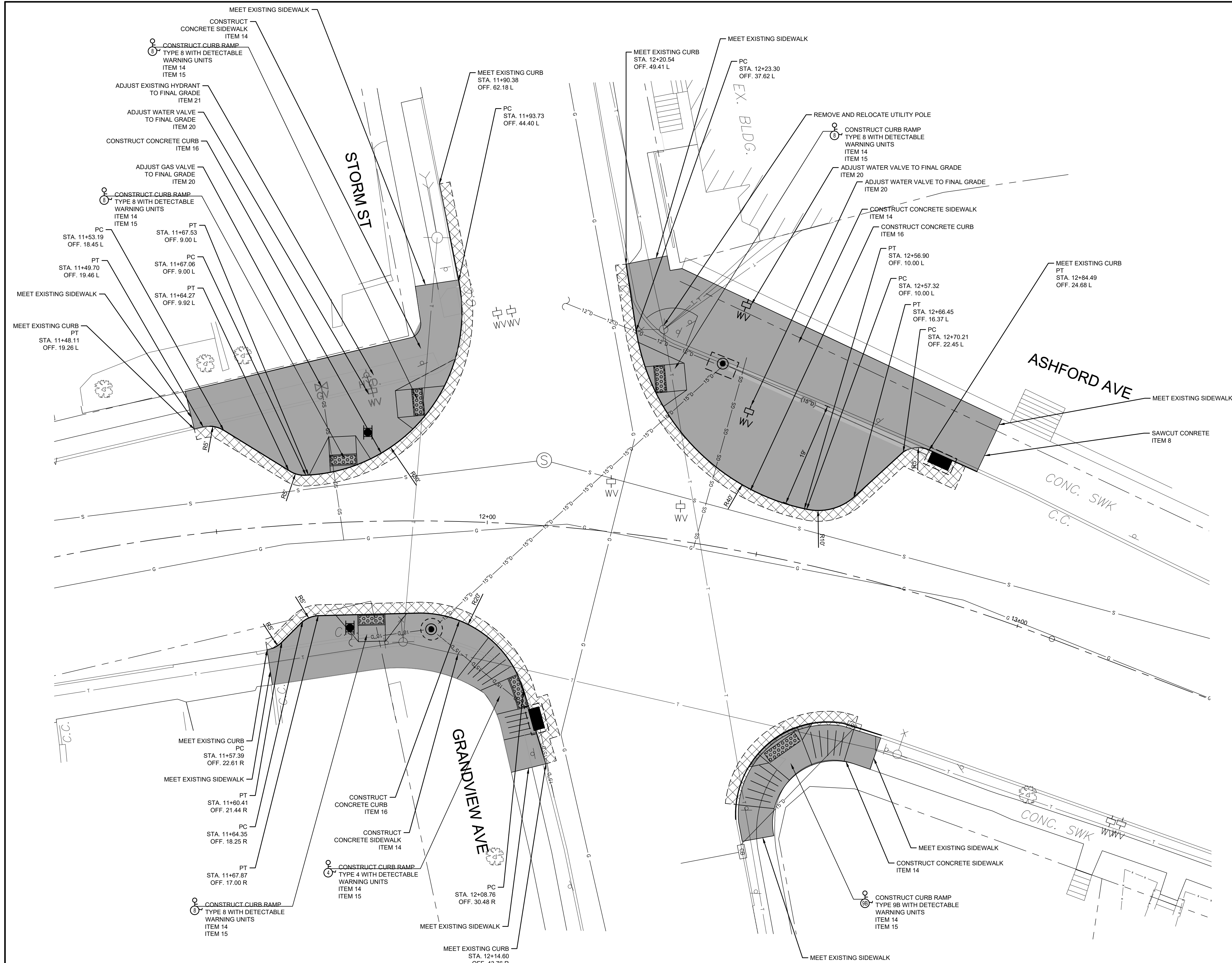
TABLE 321-02: CHANNELIZING DEVICE APPLICATION FOR SHORT-TERM STATIONARY WORK ZONES										
WORK ZONE PROVISIONS		MAXIMUM DEVICE SPACING (CENTER TO CENTER)	MUTCD COMPLIANT CHANNELIZING DEVICE							
SHORT-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.	DRUMS		STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY TUBULAR MARKERS	INTERIM TUBULAR MARKERS	VERTICAL PANELS	OVERSIZED VERTICAL PANELS	TYPE III BARRICADES
	20 FT.		X	X	X	X			X	X
NOTES: X= ALLOWED, BLANK = NOT ALLOWED										

SIGN	NON-FREEWAY	FREEWAY
G20-2	36x18	48x24
R9-11L/R9-11R**	24x18	24x18
R9-9**	24x12	24x12
W20-1	36x36	48x48
M4-9bL/M4-9bR**	30x24	30x24
WARNING FLAG	18x18	18x18

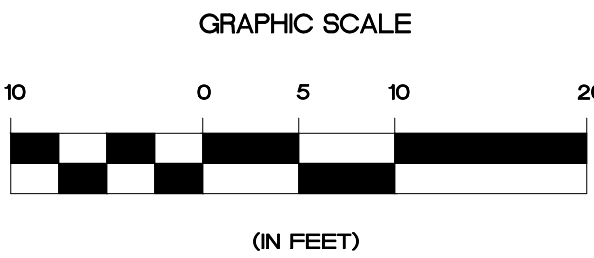
*FREEWAY FLAG MAY BE USED ON NON-FREEWAY, IF SPACE CONSTRAINTS DO NOT EXIST.
 **SIGNS NOT FOR FREEWAY USE

[illegible]

PDI: 230911-04.43 | DIR: G:\PROJECTS\23017\DWG\ROADWAY\23017_GP_4BUMPOUTS.DWG | DST: ROADWAY (PDF).PC3 | PST: _ROADWAY - STANDARD.CTB | PLS: 1:1 | BY: LW



LEGEND	
PAVEMENT RESTORATION	
CONCRETE SIDEWALK (REINFORCED)	
PROPOSED CURB	
SAWCUT	



No.	DATE	REVISION	BY:

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CONSULTANT:



NELSON + POPE

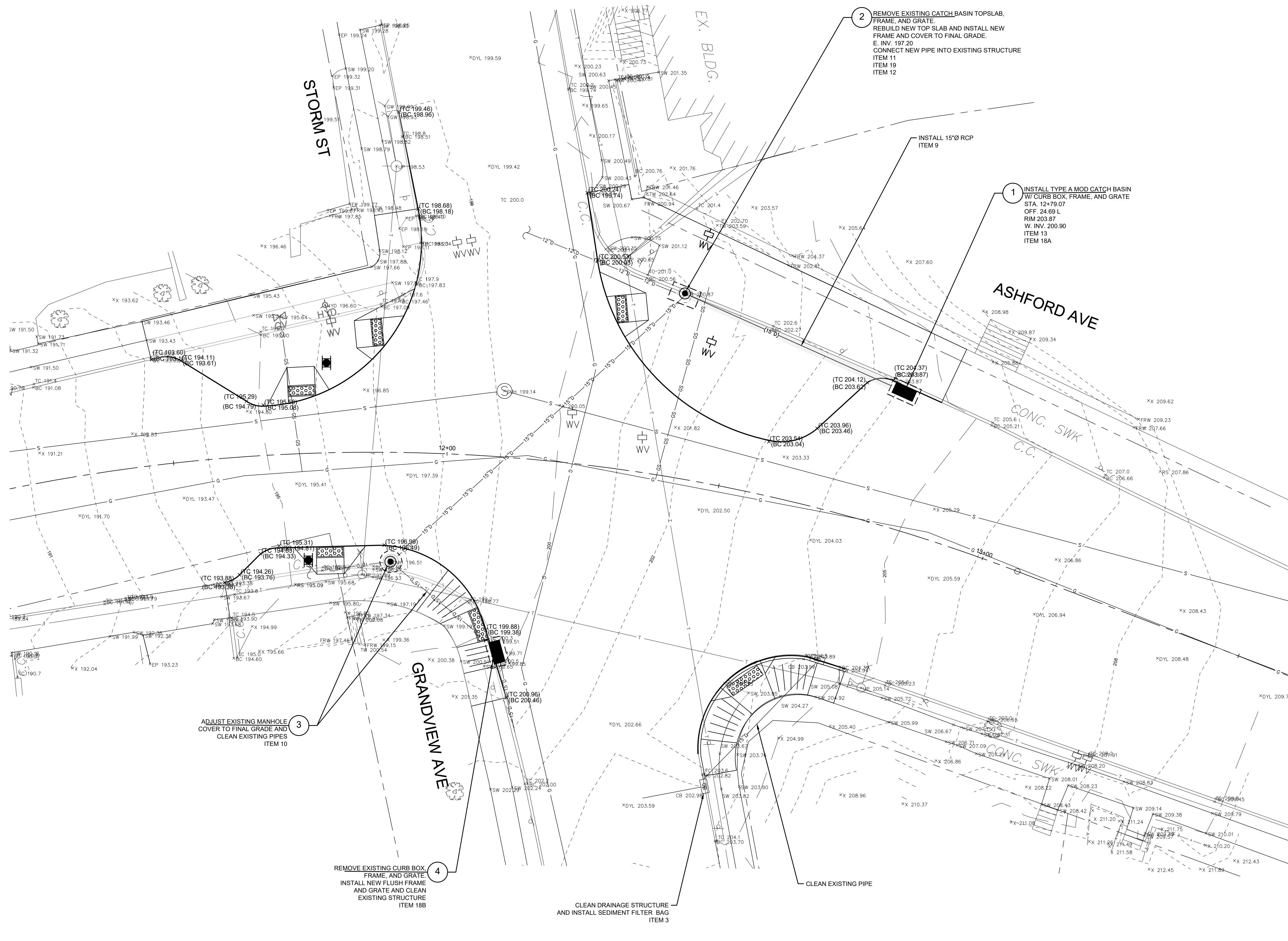
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CLIENT:	VILLAGE OF DOBBS FERRY 112 MAIN STREET DOBBS FERRY, NEW YORK 10522
DWN BY:	LW
CHK'D BY:	JD
DATE:	SEP 2023
JOB No.:	23017
CADD:	23017_GP_4BUMPOUTS
SCALE:	AS SHOWN

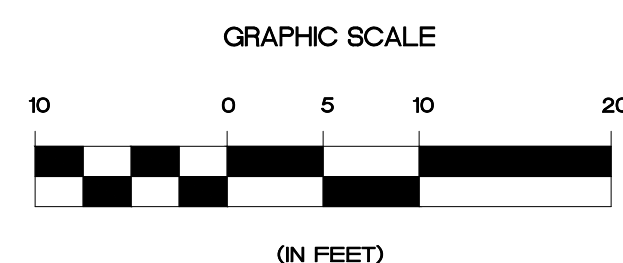
DRAWING TITLE:	CONSTRUCTION PLAN - 1 PEDESTRIAN IMPROVEMENTS AT THE INTERSECTION OF ASHFORD AVE AND GRANDVIEW AVE / STORM ST VILLAGE OF DOBBS FERRY, NEW YORK
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DRAWING NUMBER:	CP-1
SHEET:	9 OF 11



1. THE CONTRACTOR SHALL CLEAN EXISTING AND PROPOSED STRUCTURE AS NECESSARY.

<u>LEGEND:</u>	
	EXISTING SURVEY SPOT ELEVATION
	EXISTING MINOR CONTOUR
	EXISTING MAJOR CONTOUR
	PROPOSED SPOT ELEVATION
	PROPOSED DRAINAGE CATCH BASIN TYPE A-MOD
	PROPOSED DRAINAGE PIPE
	PROPOSED DRAINAGE MANHOLE



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-	-	-	-/-		 NELSON + POPE <i>engineers • architects • surveyors</i>	VILLAGE OF DOBBS FERRY 112 MAIN STREET DOBBS FERRY, NEW YORK 10522	CHKO BY: JD	DATE: SEP 2023		SHEET: 10 OF 11
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